



Notice of meeting of

Planning Committee

To: Councillors R Watson (Chair), Ayre, D'Agorne, Firth, Funnell, Galvin, Horton, Hyman, Moore, Morley, Potter (Vice-Chair), Reid, Simpson-Laing, B Watson and Wiseman

Date: Thursday, 25 November 2010

Time: 4.30 pm

Venue: The Guildhall, York

The site visit will commence at 12.30pm on Wednesday 24 November 2010 meeting at the main entrance to the Nuffield Hospital (car parking available)

AGENDA

1. **Declarations of Interest**

At this point, members are asked to declare any personal or prejudicial interests they may have in the business on this agenda.

2. **Minutes** (Pages 5 - 12)

To approve and sign the minutes of the last meeting of the Planning Committee held on 23 September 2010.

3. **Public Participation**

It is at this point in the meeting that members of the public who have registered their wish to speak can do so. The deadline for registering is by **5.00pm** on **Wednesday 24 November 2010**. Members of the public can speak on specific planning applications or on other agenda items or matters within the remit of the committee.

To register please contact the Democracy Officer for the meeting, on the details at the foot of this agenda.

4. Plans List

This item invites Members to determine the following planning applications:

- a) **Nuffield Hospital York, Haxby Road, York YO31 8TA (10/01677/FULM)** (Pages 13 - 34)

Construction of an 83 bed residential care home including ancillary facilities, external works and access [*Clifton Ward*] **[Site Visit]**

- b) **Proposed University Campus Lying Between Field Lane and Low Lane A64 Trunk road and Hull Road, York (10/02140/REM)** (Pages 35 - 44)

Erection of social and catering building with access and cycle parking. [*Heslington Ward*]

5. **Three Conservation Area Appraisals for Strensall, Strensall Railway Buildings and Towthorpe Village Conservation Areas: Consultation Drafts** (Pages 45 - 136)

This report requests that three separate draft Conservation Area Character Appraisals for the neighbouring conservation areas of Strensall, Strensall Railway Buildings and Towthorpe Village are approved for public consultation.

6. **Any other business, which the Chair considers urgent under the Local Government Act 1972.**

Democracy Officer:

Name: Jill Pickering

Contact Details:

- Telephone – (01904) 552061
- E-mail – jill.pickering@york.gov.uk

For more information about any of the following please contact the Democracy Officer responsible for servicing this meeting:

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- Business of the meeting
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PLANNING COMMITTEE

SITE VISIT

WEDNESDAY 24 NOVEMBER 2010

Meet on site at the main entrance to the Nuffield Hospital (car parking is available)

TIME (Approx)	SITE	ITEM
12.30pm	Nuffield Hospital York, Haxby Road, York (10/01677/FULM)	4a

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City of York Council

Committee Minutes

MEETING

PLANNING COMMITTEE

DATE

23 SEPTEMBER 2010

PRESENT

COUNCILLORS R WATSON (CHAIR), AYRE, D'AGORNE, FIRTH, FUNNELL, HYMAN, MOORE, POTTER (VICE-CHAIR), REID, SIMPSON-LAING, B WATSON, WISEMAN AND GALVIN (SUB FOR CLLR HUDSON)

APOLOGIES

COUNCILLORS HORTON, HUDSON AND MORLEY

11. DECLARATIONS OF INTEREST

Members were invited to declare, at this point in the meeting, any personal or prejudicial interests they might have in the business on the agenda.

Councillor Ayre declared a personal non prejudicial interest in relation to Plans items 4a (6-18 Hull Road, York) and 4b (32 Lawrence Street, York) as Executive Member for Leisure, Culture and Social Inclusion.

12. MINUTES

RESOLVED: That the minutes of the last meeting of the Planning Committee held on 22 July 2010 be approved and signed by the Chair as a correct record.

13. PUBLIC PARTICIPATION

It was reported that there had been no registrations to speak at the meeting under the Council's Public Participation Scheme.

14. PLANS LIST

Members considered reports of the Assistant Director (Planning and Sustainable Development) relating to the following planning application, outlining the proposals and relevant planning considerations and setting out the views of the consultees and officers.

14a 6-18 Hull Road, York (10/01743/OUTM)

The Committee considered a major outline application, submitted by Uniliving Ltd, for the erection of student accommodation comprising of 282 bed spaces within 75 unit clusters in 5 blocks with associated landscaping and access after the demolition of the existing dairy (resubmission).

Officers circulated the following information in an update (full details of which are set out in the annex attached to the agenda):

- Two further letters of objection had been received from residents.
- Comments had been received from the Authority's Sustainable Project's Officer supporting the proposals subject to prior approval of a renewable energy strategy.
- No commitment to the Institute of Civil Engineers Demolition Protocol had been received from the applicant but he had now confirmed his commitment to the protocol.
- Additional archaeology comments had been received which sought the replacement of draft conditions 14 and 15 for one bespoke condition for the site.
- Amendment of draft condition 7 to also include details of the stack height.

Representations in support of the application were received from the applicant's agent. He referred to extensive negotiations undertaken with Officers since refusal of the application in June, which had involved a new design approach and a reduction in bed spaces. He referred to the large amount of private student accommodation in the Hull Road/Fishergate Ward and pointed out that this development would release around 100 family dwellings onto the market. The environmental management plan and permanent on-site management presence would be confirmed reduce noise and anti social behaviour issues in a highly sustainable location.

Representations in objection were received from a local resident who referred to the rapid spread of the University campus and subsequent loss of family housing to students. He pointed out that local residents and Parish Councils felt that the University should provide student accommodation on campus.

The Local Member also pointed out that the University had failed to build sufficient accommodation on campus. He referred to residents concerns in relation to the earlier scheme and confirmed that he had no objections to the present application just a number of questions. These related to the green wall screening, parking and the strength of draft condition 24 which should include dark sky compliant lighting.

In answer to Members questions the applicant's Architect confirmed that Uniliving had a similar occupancy management plan in operation in a development in Hull which had worked well in maintaining control of occupants.

Members went onto question a number of aspects of the scheme including;

- Details of the green wall screening;
- Confirmation that the development was not tied to a specific University;
- Letting of accommodation out of term time;
- Details of bus routes using the proposed new bus shelter;
- Confirmation that the site would have a 24 hour management presence;

- Confirmation that discussions with the Police Architectural Liaison Officer would be undertaken at the reserved matters stage;
- Open Space contribution and its use in the locality rather than at the University

Following further lengthy discussion it was

RESOLVED: i) That the application be approved subject to the conditions set out in the report and the following amended conditions:

Amendment of Condition 7 to read “Notwithstanding the information contained on the approved plans, the height of the approved development shall not exceed 14.8 metres, to ridge height and 15.8 metres to maximum stack height, as measured from existing ground level. Before any works commence on the site, a means of identifying the existing ground level on the site shall be agreed in writing, and any works required on site to mark that ground level accurately during the construction works shall be implemented prior to any disturbance of the existing ground level. Any such physical works or marker shall be retained at all times during the construction period.”

Deletion of Conditions 14 and 15 and their replacement with “No demolition shall take place until the applicant has secured the written approval of the Local Planning Authority for the content and implementation of an archaeological mitigation strategy which consists of: demolition and level reduction to the top of surviving archaeological deposits under archaeological supervision; archaeological excavation of features and deposits associated with the 19th Century tannery and its subsequent development; analysis, reporting and publication of the results of the excavation; deposition of the archive with the Yorkshire Museum; an archaeological watching brief on all other ground disturbances; and community access and involvement in the project.”

Condition 31: That the Chair and Vice Chair in consultation with the Assistant Director of Planning and Sustainable Development be authorised to agree the details of the siting of the proposed bus stop on Lawrence Street.

- ii) That under Section 106 of the Town and Country Planning Act 1990 the financial contribution towards off site provision of open space should specify that the contribution should be used for provision in the locality of the site and not at the University.

REASON: In the opinion of the Local Planning Authority the proposal, subject to the conditions listed, would not cause undue harm to interests of acknowledged importance, with particular reference to the principle of the development, affordable housing /occupancy, impact upon the character and appearance of the area, impact upon residential amenity, living conditions of future occupants, parking and highway issues, open space issues archaeological significance of the site and sustainability. As such the proposal complies with Policies ED10, GP1, GP4a), GP6, H1, HE10, L1c) and T4 of the City of York Development Control Local Plan.

14b 32 Lawrence Street, York (10/01359/FULM)

Consideration was given to a major full application, received from Blacklion Ltd, for the erection of 6 no. blocks for student accommodation after the demolition of an existing car showroom (resubmission).

Officers circulated an update, which informed Members of the following (the full updated is attached to the online agenda):

- The actual number of units was 237 rather than 238
- English Heritage had no further comments in relation to the amended scheme
- 8 further letters of objection had been received covering a number of new points
- St Lawrence Church had withdrawn its objection to the scheme subject to a number of points
- York Civic Trust had continued concerns regarding the density, scale and massing and particularly the design of the buildings
- Further comments of the Environmental Health Officer including a request for the addition of a noise condition
- Highway request for amendment to draft condition 18 to relate to the provision of cycle parking
- Lifelong Learning and Leisure had confirmed that the commuted sum for off site open space would be a total of £76,432. They had also confirmed that there was a deficiency of sport provision within the area and that the money would be spent as part of the community accessible facilities at Heslington East.
- Following receipt of additional drainage information the Engineering Consultancy had requested the addition of a number of additional conditions and an informative note
- Details of the plan numbers for inclusion in draft condition 2.

Representations in support of the application were received from the Planning Consultant. He thanked Officers for their assistance with the application and pointed out that the site was in a central location making it ideal for student accommodation. He stated that the provision of

accommodation in the city had not kept pace with student numbers and that the site would be covered by a management plan. He went on to refer to a number of concerns raised during the site visit in relation to car parking and he reiterated that highways had raised no objections to the proposals.

Representations were received in objection from a local resident who pointed out that the plans displayed at the meeting were not the same as the plans he had purchased from the Authority when the application had been submitted.

The Planning Officer confirmed that, since the application had been submitted, a number of revisions had been made to the scheme on which residents had been reconsulted. Officers detailed the major changes made to the scheme since it had originally been lodged.

The local resident went on to express concerns regarding the proposed height of the student accommodation blocks and to the gap left in fencing adjacent to Lawrence Lane and the site access. He also referred to drainage issues in an area where the drains were already overloaded.

Representations in objection were also received from an Osbaldwick resident who expressed concern at the pressure being put on Green Belt land for housing development. He referred to the high level of objections to the proposals for the site, which he hoped Members would note. He again felt that student accommodation should be provided within the University campus.

Representations were then received from a representative of the York Civic Society. The representative stated that the Society had a number of concerns including that this development was purely speculative and had not been requested by the University. He stated that, in their opinion, the present scale, design and materials were not suitable for the area. The access road would be sited against the Ellen Wilson Homes and affect the amenity of vulnerable residents.

The Local Member expressed concerns at the proposals particularly in relation to its effect on the Conservation Area and the adjacent listed buildings. He felt that there was insufficient car parking and the massing would also have a detrimental impact on the Tannery and Barbican Mews. He requested the Committee to refuse the application.

Members went on to question a number of aspects of the application and areas of concern, which included:

- Width between the accommodation blocks for emergency services, which it was confirmed would be covered by building regulations
- Use of the open space contribution at the University
- Further details of the occupancy management plan
- Lawrence Lane access to site via keypads and confirmation that this would also include CCTV both inside and outside the properties
- Parking provision and potential parking displacement in the surrounding area

In answer to Members questions Officers confirmed that it was not necessary to specify where the contribution for off site open space should be used. It was pointed out that Officers in Life Long Learning and Leisure would take account of Local Plan policies and direct monies to any identified needs in the area.

Following further lengthy discussion Councillor D'Agorne moved and Councillor Potter seconded refusal of the application on the grounds of overdevelopment of the site, its impact on the Conservation Area and its affect on the amenity of adjacent residents. On being put to the vote it was

RESOLVED: That the application be refused.

REASON: 1. It is considered that as a consequence of their location and density as well as their excessive scale, height and massing, the proposed student blocks, would be detrimental to the character and appearance of the Central Historic Core Conservation Area and the setting of adjacent listed Ellen Wilson Almshouses and St Lawrence Church. The development is therefore contrary to advice in Planning Policy Statement 5 'Planning and the Historic Environment' and the accompanying Historic Environment Planning Practice Guide (March 2010), which highlight the importance of the protection of heritage assets, and to the following – Development Control Local Plan (Approved April 2005) policies:-

GP1 (Design) which states, inter alia, that development proposals will be expected to:

- a) respect or enhance the local environment;
- b) be of a density, layout, scale, mass and design that is compatible with neighbouring buildings, spaces and the character of the area, using appropriate building material;

HE2 (Development in Historic Locations) which states that within or adjoining conservation areas and in locations which affect the setting of listed buildings, scheduled monuments or nationally important archaeological remains (whether scheduled or not), development proposals must respect adjacent buildings, open spaces, landmarks and settings and have regard to local scale, proportion, detail and materials;

HE3 (Conservation Areas) which states development proposals will be expected to:

- a) respect or enhance the local environment;
- b) be of a density, layout, scale, mass and design that is compatible with neighbouring buildings, spaces and the character of the area, using appropriate building materials.

HE4 (Listed Buildings) which states consent will only be granted for development in the immediate vicinity of listed buildings where there is no adverse effect on the character, appearance or setting of the building (s).

2. It is considered that as a consequence of their location and density as well as their excessive scale, height and massing, the proposed student blocks, would dominate and overlook existing residential development to the south, west and east of the site, and would therefore be detrimental to the residential amenity of adjacent dwellings. This is contrary to Development Control Local Plan (Approved April 2005) Policy GP1 (Design), which states that development proposals will be expected to, inter alia:-

i) ensure that residents living nearby are not unduly affected by noise, disturbance, overlooking, overshadowing or dominated by overbearing structures.

15. APPEALS PERFORMANCE AND DECISIONS SUMMARY

The Committee considered a report, which informed them of the Council's performance in relation to appeals determined by the Planning Inspectorate in the 3-month period up to 30 June 2010. The report also provided a summary of the salient points from appeals determined in that period together with a list of outstanding appeals as at 31 July 2010.

Members expressed their thanks to the Planning Officers for efforts in this area.

RESOLVED: That the reports content be noted.

REASON: So that members can continue to be updated on appeal decisions within the CYC area and informed of the planning issues surrounding each case for future reference in determining planning applications.

R WATSON, Chair

[The meeting started at 4.30 pm and finished at 6.45 pm].

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COMMITTEE REPORT

Date: 25 November 2010 **Ward:** Clifton
Team: Major and Commercial Team **Parish:** Clifton Planning Panel

Reference: 10/01677/FULM
Application at: Nuffield Hospital York Haxby Road York YO31 8TA
For: Construction of 83 bed residential care home including ancillary facilities, external works and access
By: Primary Care Investments Ltd
Application Type: Major Full Application (13 weeks)
Target Date: 2 November 2010
Recommendation: Approve

1.0 PROPOSAL

1.1 This is a full application for the erection of a new, 83 bedroomed, residential nursing care home at land adjacent to Nuffield Hospital, Haxby Road York

1.2 The site is an area of 0.46 ha located on the east side of Haxby Road. Access to the site is from Haxby Road via the access that serves the Nuffield Hospital. The site is currently used as a car park and has been leased out to the NHS hospital during the alterations/improvements that have been taking place at that site. To the north, east and south of the site is existing residential development and to the west is the car park for the Nuffield Hospital. Conservation area no.35 The Nestle/ Rowntree factory is located to the west and incorporates the majority of the Nuffield Hospital Buildings and the front part of the Nuffield Hospital car park.

1.3 The proposed development is designed to provide two and three storey communal and bedroom accommodation for a residential care home with ancillary facilities on a fourth floor. The care facility includes a 10 bedroomed young physically disabled (YPD) area at ground floor level. The building presents an L-shaped structure to the North-west of the site enclosing a 19 space car parking area, 16 space secure cycle parking and access arrangements. The north wing is two storeys with a flat, parapet roof, the west wing, is three storeys with ancillary facilities provided over part of the flat roofed area creating a fourth floor. A further wing to the building extends east wards at three storeys. The total internal floor area of the structure is 4,091 square metres.

1.4 The application has been amended since first submission in the following ways:-

-increase in separation distance between northern end of the building and the boundary.

- Alterations to the design and size of the top storey of the building

- Additional landscaping to the western boundary and south-eastern boundary adjacent to garages
- Visitor cycle hoops to entrance
- Relocation of drainage runs from under tree canopies
- Adjustment of external materials

1.5 The application is supported by a Design and Access statement incorporating a Sustainability Statement, Geo-Environmental report, Tree survey and Arboricultural report and Topographical survey.

2.0 POLICY CONTEXT

2.1 Development Plan Allocation:

City Boundary GMS Constraints: York City Boundary 0001

DC Area Teams GMS Constraints: East Area (2) 0005

Floodzone 2 GMS Constraints: Flood zone 2

Schools GMS Constraints: Haxby Road Primary 0198

2.2 Policies:

CYH17

Residential institutions

CYGP1

Design

CYGP4A

Sustainability

CYGP9

Landscaping

CYHE2

Development in historic locations

CYNE1

Trees, woodlands, hedgerows

3.0 CONSULTATIONS

INTERNAL

3.1 Highway Network Management - Expected levels of traffic generation are low and are unlikely to have a material impact on the surrounding highway

network. The access arrangements are considered suitable to serve the level of development proposed. Although the site will employ 80-90 staff due to staff shift patterns the expected maximum staff on site at any one time is approximately 15 to 20. Car parking levels are therefore realistic and will be supported by the aims of a travel plan which will further reduce car borne trips generated by the site.

3.2 City Development - relevant policies are H17, HE2, HE4, GP4a and NE1. There are no policy objections to the application provided housing and adult services are supportive of the principle of the development. Officers should be satisfied with the design, heritage, landscape and sustainability considerations.

3.3 York Consultancy - The development is within flood zone 1 and is unlikely to suffer from river flooding. Object to the application which provides insufficient and conflicting information about drainage.

3.4 Countryside Officer - The development presents a good opportunity to carry out some habitat enhancement work to benefit a range of species. The enhancement works should include features included within the design of the building such as bat bricks and tiles. A condition is suggested to ensure that a habitat enhancement is achieved as part of the development.

3.5 Design and Conservation - The proposal represents an intensive development of the site. The built form whilst not directly impacting on the setting of the conservation area presents a functional west elevation which will be visible from the conservation area. The screen planting proposals to this elevation need to be clearly understood, and should be covered by an appropriate condition. The external materials proposed for the development are a mix of brick, render and metal cladding panels. Although these are the materials found on the adjacent Nuffield Hospital - the former recreation block to the Rowntree factory complex - the design intent still needs to be clear. Revisions to the scheme have increased the amount of facing brick which is welcome, but the external materials should still be covered by an appropriate condition. The external amenity space looks okay on plan, in reality the depth will feel smaller because of the more powerful vertical presence of the trees and the shade they will cast due to their location on the southern boundary. Nonetheless the physical distance between trees and buildings is acceptable. Because of the tightness of the development to the existing landscape margins appropriate construction phase protection and the subsequent landscape improvements to the perimeter of the site will need to be carefully controlled by condition.

3.6 Engineering Consultancy - The site is in flood zone 1 and should not suffer river flooding. Insufficient and conflicting information has been provided to determine the potential impact the proposal may have on the existing drainage systems. Engineering consultancy therefore currently object to the application.

3.7 Environmental Protection - No objections in principle subject to extensive conditions covering protection of surrounding properties during the

construction of the building through a Construction Environmental Management plan, ensuring noise from plant and machinery is at an acceptable level, protection of residents from road noise, controlling odour from kitchens, contamination and levels of external lighting.

3.8 Sustainability Officer – Comments awaited

EXTERNAL

3.9 Clifton Ward Planning Panel - The size of the building footprint and the bulk of the building raises some concern because of the impact of the development on the neighbouring houses to the south and the east, although it is considered that a good attempt has been made to reduce the overall appearance of the development through the elevational treatment of the building. Concerned about overshadowing; there appeared to be no sun-path diagram in the documents, this might be especially relevant to properties on the north side of the development. Question the size of the cycle store and the ability to expand this area if necessary. It is assumed that the care staff will be the main users of cycle places. The panel question whether 19 parking spaces will be sufficient for the number of beds and staff.

3.10 Police Architectural Liaison Officer - Analysis of police recorded incidents show no significant issues related to crime and the location of the site. The design and access statement demonstrates that crime prevention has been considered. The applicant is encouraged to apply for the Police secure by design award. There are no concerns regarding crime and community safety in relation to this development.

3.11 Environment Agency - No objections provided a condition is imposed to ensure that surface water drainage details, based on sustainable drainage principles, is conditioned.

3.12 2 letters of objection have been received covering the following points:-

- There were tunnels under the car park do these tunnels still exist
- The building covers the greater part of the land area and is intrusive to surrounding residential properties, impacting on the privacy of individuals.
- The proposal is effectively a three storey structure, the surrounding properties are only two; the building will overwhelm by its size and construction
- Reduce natural light to surrounding properties
- Parking will be a problem. The public rely less on public transport than the council would have us believe
- There are better sites for this scheme such as Vickers Instruments site on Haxby Road.
- The block is very high and near the back of properties on Kitchener Street
- Noise levels will increase because the proposal is a 24 hour operation
- There is already a parking problem in Kitchener Street this will be made worse by the development

Publicity

3.13 The application has been advertised by means of a site notice posted on the 17th August 2010, a press advert dated 28th August 2010 and via neighbourhood notification letters.

4.0 APPRAISAL

4.1 Key Issues:-

- Policy Background
- Principle of the development
- Design and Landscaping
- Highways, access and parking
- Residential Amenity
- Policy H17 considerations
- Ecology
- Drainage
- Sustainability

4.2 The proposal is for an 83 bedroomed care home, 10 of the beds are envisaged to accommodate young physically disabled (YPD).

4.3 Government guidance in Planning Policy Statement 1 ("Delivering Sustainable Development") (PPS1) states that a number of key principles should be applied to ensure that decisions taken on planning applications contribute to the delivery of sustainable development. In particular, PPS1 promotes high quality inclusive design in the layout of new developments and individual buildings in terms of function and impact, not just for the short term but over the lifetime of the development. Design which fails to take the opportunities available for improving the character and quality of an area should not be accepted. High quality and inclusive design should create well-mixed and integrated developments which avoid segregation and have well planned public spaces that bring people together and provide opportunities for physical activity and recreation.

4.4 Planning Policy Statement 4 (PPS4) 'Planning for Sustainable Economic Growth' says that economic development includes public and community uses and that policies in the PPS also applies to other development that either provides employment opportunities, generates wealth or produces or generates an economic output. Policy EC10 of PPS4 says that Local Planning Authorities should adopt a positive and constructive approach towards planning applications for economic development. All development should be assessed against the following impact considerations:-

- a. whether the proposal has been planned over the lifetime of the development to limit carbon dioxide emissions, and minimise vulnerability and provide resilience to, climate change
- b. the accessibility of the proposal by a choice of means of transport including walking, cycling, public transport and the car, the effect on local traffic levels and congestion (especially to the trunk road network) after public transport and traffic management measures have been secured

- c. whether the proposal secures a high quality and inclusive design which takes the opportunities available for improving the character and quality of the area and the way it functions
- d. the impact on economic and physical regeneration in the area including the impact on deprived areas and social inclusion objectives
- e. the impact on local employment

4.5 Planning Policy Statement 5 - 'Planning and the Historic Environment' says the Government's overarching aim is that the historic environment and its heritage assets should be conserved and enjoyed for the quality of life they bring to this and future generations. To achieve this the Governments objectives for planning for the historic environment are to deliver sustainable development, to conserve England's heritage assets in a manner appropriate to their significance and to contribute to our knowledge and understanding of our past by ensuring that opportunities are taken to capture evidence from the historic environment and to make this publicly available.

4.6 PPS5 is supported by a practice guide which provides more detailed advice on how to deal with applications in an historic environment. In particular paragraph 80 says policies HE7.5, HE9.5 and HE10 require attention to the extent to which the design of new development contributes positively to the character, distinctiveness and significance of the historic environment. The paragraph acknowledges that a successful scheme will be one whose design takes into account sites historical features including views into and from the site and surroundings and green landscaping.

4.7 PPS9 'Biodiversity and Geological Conservation' says development proposals provide many opportunities for building-in beneficial biodiversity or geological features as part of good design. When considering proposals, Local Planning Authorities should maximise such opportunities in and around developments, using planning obligations where appropriate.

4.8 The following local plan policies are considered to be relevant to the application:-

- Policy H17 which states that planning permission will only be granted for residential institutions where the development, together with existing residential institutions or unimplemented planning permissions for that use, would not give rise to a concentration likely to have an adverse impact on residential amenity and where it is positively located relative to local facilities and public transport.
- Policy C1 says that planning applications for social, health, community and religious facilities will be granted provided that the proposed development is of a scale and design appropriate to the character and appearance of the locality and it would meet a recognised need.
- Policy GP1 'Design' includes the expectation that development proposals will, inter alia; respect or enhance the local environment; be of a density, layout, scale, mass and design that is compatible with neighbouring buildings and spaces, ensure residents living nearby are not unduly affected by noise,

disturbance, overlooking, overshadowing or dominated by overbearing structures, use materials appropriate to the area; avoid the loss of open spaces or other features that contribute to the landscape; incorporate appropriate landscaping and retain, enhance or create urban spaces, public views, skyline, landmarks and other features that make a significant contribution to the character of the area.

- Policy GP4a 'Sustainability' of the City of York Council Development Control Local Plan (2005) states that proposals for all development should have regard to the principles of sustainable development and sets out those issues to consider as part of a sustainably designed development. The interim planning statement on Sustainable Design and Construction supports Policy GP4a in setting out ways to achieve sustainability furthermore the document requires that 10% onsite renewables are achieved.
- Policy GP9 requires where appropriate developments to incorporate a suitable landscaping scheme
- Policy HE2 'Development in Historic Locations' says within or adjoining conservation areas, and in locations which affect the setting of listed buildings development proposals must respect adjacent buildings, open spaces, views, landmarks and other townscape elements which contribute to the character or appearance of the area.
- Policy NE1 requires that trees, woodlands and hedgerows which are of landscape, amenity, nature conservation or historical value will be protected.

PRINCIPLE OF THE DEVELOPMENT

4.9 The site is located within the built area of the city as defined in the City of York Draft local plan. The site has previously been used for industrial purposes in connection with the former use of the site by Nestle/Rowntrees and is currently used as a car park. Originally allocated for employment use the site has not been included as employment land within the employment land review and its retention as employment land is not required. PPS4 says Local Planning Authorities should be supportive of economic development subject to the detailed design of the scheme. There are no policies in the Local Plan that would preclude the use of the site for a residential care home subject to the details of the scheme being acceptable. The principle of the development is therefore considered to be acceptable.

DESIGN AND LANDSCAPING

4.10 The proposed building is predominantly 3 storey with a significant 4 storey element housing the ancillary accommodation and a two storey north wing. The site is currently used as a car park. Nuffield Hospital, which fronts the Haxby Road to the west of the application site, occupies the former Rowntree staff welfare/canteen building. The hospital building is an imposing red brick building with a tall and detailed parapet with later additions to the roof storey in grey metal clad set behind. The adjoining theatre building is listed and the Nestle Rowntree Conservation Area includes these buildings

and the Haxby Road frontage. The proposed form of the development attempts to reflect the form of the Nuffield Hospital using facing brick/render panels with a parapet and flat roof to the 2/3 storey elements and metal cladding to express the vertical circulation cores and to the 4th storey element which has a pitched roof. The design and access statement suggests that the building adopts the form of the industrial buildings of the Rowntree complex and specifically the Nuffield hospital building.

4.11 Both PPS1 and PPS4 says that new developments should secure high quality and inclusive design which takes the opportunities available for improving the character and quality of the area and the way it functions. Policy GP1 of the Local Plan includes the expectation that development proposals will respect or enhance the local environment and be of a density, layout, scale, mass and design that is compatible with neighbouring buildings and spaces. PPS5, in the accompanying practice guide acknowledges that the views into and out of the conservation area are an important consideration.

4.12 The application site has a varied visual context; the Haxby Road frontage has distinctive industrial design associated with the former use of the area by the Rowntree factory. The north, south and east is domestic scale development of varying ages. The design of the scheme very much aligns itself with the design of the industrial qualities of the surrounding development and there is as much legitimacy for this approach as for the alternative approach of a more domestic design approach. In the context of policies the issue is whether the design undermines the character and qualities of the area, respects the local environment or detracts from views into or out of the conservation area.

4.13 The building is set back from Haxby Road and from this vantage point the main visual references are the existing former industrial buildings (now Nuffield Hospital). In this context the proposed building will not, in officer's view, be out of character with its surroundings. The domestic properties to the north, east and south are less visually related to the site because of the planting to the site boundaries and the intervening rear access roads. Taking reference for the design from these structures is not necessarily a more legitimate approach than that taken by the architect and may make it difficult for the applicant to achieve the level of floor space required on site. In Officer's opinion the scheme has evaluated its surroundings and identified a design that has regard to the character and qualities of the area. Subject to the conditions as suggested by the Conservation Officer the form of development would be it is felt be appropriate for the site.

4.14 The application is supported by a landscaping scheme and Arboricultural report. The Landscaping scheme seeks to retain existing boundary planting and to enhance this planting. New trees are proposed to the west and eastern boundaries and existing hard surfacing around the new building is to be replaced with grassed areas, new shrub planting and patio areas. The application has been amended since first submission to pull the building away from planting on the northern boundary, incorporate more trees in to the car park area and to the east and west boundaries. Drainage runs have been relocated from under tree canopies. Protective fencing is proposed during the

construction period. The alteration of the building on the northern side overcomes some of the concerns of the Landscape Architect. However some concern remains about the overshadowing of the main garden area to the south of the care home by existing trees and hence the usefulness of the garden as an outside space. The applicant's agent takes a different view on this garden area. In their experience the garden will not be used significantly by residents, but residents will be able to look out at the trees from the communal areas and bedrooms providing them with a pleasant outlook.

4.15 There are no planning policy requirements for minimum garden areas, garden orientation or provision of outdoor facilities for care homes. Nonetheless, the landscaping both existing and proposed and the space around the development will provide a good setting for the building and Officers are satisfied with the principles of the proposed landscaping scheme. Conditions are proposed to ensure that existing planting is protected during construction and that the landscaping scheme submitted is properly detailed and implemented.

HIGHWAYS, ACCESS AND PARKING

4.16 Access to the development is from Haxby Road via an existing access which also serves Nuffield Hospital. The proposal includes 19 car parking spaces, 16 covered and secure cycle spaces and three Sheffield hoops for visitor cycle parking. The parking area is located on the north-west side of the building adjacent to the former sweet shop building used by Rowntree and now housing plant associated with the Nuffield hospital.

4.17 The supporting information with the planning application is based upon nationally recognised TRICS data and the applicant's experience of running a number of these types of homes. From this information it can be ascertained that the expected levels of traffic generation are low, in the region of 8 movements during the peak network periods, and are unlikely to have a material impact on the surrounding highway network. Access to the site is via the existing junction onto Haxby Road, which is also used by the Nuffield Hospital. The access arrangements are considered suitable to serve the level of development proposed.

4.18 The site will employ in the region of 80 to 90 staff; however due to staff shift patterns and the differing nature of roles (clerical/nurses/maintenance etc) the expected maximum number of staff on site at any one time is approximately 15 to 20. Car parking levels are therefore realistic and will be supported by the aims of a travel plan which will further seek to reduce car borne trips generated by the site.

4.19 The site can be considered to be in a sustainable location and is well served by bus stops served by frequent public transport services on Haxby Road, footways and cycle ways.

4.20 Highway Network Management raise no objections to the application subject to conditions which ensure the proper implementation of the development, cycle parking provision and travel plan.

RESIDENTIAL AMENITY

4.21 Existing residential development is located to the north, south and east of this proposed site. To the south are two storey Victorian terrace properties located on Kitchener Street. The properties are separated from the site by a rear alley approximately 3 metres wide. To the west, located on Huntington Road are more modern terraced properties. The properties have a shared vehicular access to the rear and are at a lower level than the application site. To the north at the rear of Haley's Terrace there is a pair of semi-detached properties. All the surrounding properties have main rear elevations facing the site.

4.22 The foot print of the building is such that to the north there will be a two storey end elevation of the building facing the boundary at a distance of between 7 and 8.5 metres. This end elevation, which is 16.5 metres wide and has within it two bathroom windows, a ground floor door and first floor stairway windows, is two storey height with maximum height of 6.6 metres. The distance between the rear face of the properties to the north and the building will be approximately 16.5 metres. To the southern boundary one of the main elevations of the building is proposed. This elevation runs 47 metres parallel with the boundary, the main part of the elevation is 3 storeys high but also there is a fourth storey provided on the roof which joins in to the main structure along the elevation so that along the length of the elevation the height of the building varies between 9.5 metres and 10.7 metres. The remaining part of the fourth storey is set back from the roof line by 1 to 2 metres and itself stand 2.1 metres high to eaves and 3.6 metres high to apex. The main bedroom windows face this boundary as well as some of the communal areas. The building is between 11 and 13 metres from the southern boundary with a single storey element and first floor balcony being 8 metres from the boundary. The distance between properties on Kitchener Street and the main face of the building is between 23 and 25 metres (this discounts the ground floor outriggers to the properties). To the western boundary the end elevation of the eastern wing of the building faces the site at a distance of between 11.5 and 12 metres, the elevation stands 9.5 metre high and is 11.5 metres wide. The distance between the properties and the building on this side is between 24 and 26 metres.

4.23 From the above analysis the distance on all boundaries between the building and the properties is considered to ensure the amenity of the residential properties will not be unduly affected by the development; furthermore all boundaries are reasonably well screened by existing planting, and to the east and south the site is separated from the houses by intervening access roads. GP1 of the Local Plan requires that developments should ensure residents living nearby are not unduly affected by noise, disturbance, overlooking, overshadowing or dominated by overbearing structures. Whilst it is clear that the structures will be visible to adjacent residents and that the overall outlook from the rear of their properties will be changed, the details of the scheme are considered to provide an acceptable relationship between existing and proposed development sufficient to accord with the requirements of GP1.

4.24 Environmental Protection are not objecting to the application but require a number of condition relating to the detail of the development to ensure neighbours are not affected by the development. Issues covered by condition include Construction Environmental Management plan, noise from plant and machinery, internal noise levels, odour from kitchens, contamination and external lighting.

POLICY H17 CONSIDERATIONS

4.25 Policy H17 only supports residential institutions where there development would not give rise to a concentration likely to have an adverse impact on residential amenity and where it is positively located relative to local facilities and public transport. Officers have examined appeal cases where this issue has been considered and it appears that there is little support for refusal on the basis of a lack of local facilities. In the main this is because the registering authority for care homes (Care Quality Commission) considers this issue through the licensing process. Consideration of the issue through the planning process would, therefore, merely duplicate controls that already exist under licensing legislation. Furthermore paragraph 65 of circular 3/2005 states that in relation to care homes that have to be registered, registration can be refused on the grounds that the home would not provide adequate services or facilities reasonably required by residents or patients. However in the case of this development the site is close to Haxby Road which is well served by public transport and it is a short walk down the road to local shops. The care home location is considered to be well located in terms of local facilities. City Development is not objecting to the principle of the development and has not identified a concentration of care home schemes within the area. The application is considered to accord with the requirements of Policy H17.

BIODIVERSITY

4.26 The Countryside Officer acknowledges that the site as existing provides little of nature conservation value. However the development provides opportunities to provide wildlife habitat enhancement works. The design of the building should include measures for habitat enhancement especially for bats and birds. Use of British native landscaping within the planting scheme will also help increase the wildlife value of the site. The requirements of the Countryside Officer are legitimate planning considerations within the context of PSS 9, and a condition is proposed to ensure the measures suggested are incorporated in to the design and landscape for the development.

DRAINAGE

4.27 The site is in flood zone 1 and should not suffer river flooding. Engineering Consultancy considers that insufficient and conflicting information has been provided to determine the potential impact the proposal may have on the existing drainage systems. Whilst it is considered that there has been enough information submitted to show that the site can be drained and that outstanding issues can be subject to conditions, an update will be provided to

committee providing details of conditions and any additional information submitted by the applicant to address the concerns of raised.

4.28 The Environment Agency is not objecting subject to a sustainable drainage condition being imposed on any permission granted.

SUSTAINABILITY

4.28 The application is supported by a sustainability statement which commits to the principles set out in Policy G4a of the Local Plan and the IPS on sustainable design and construction. It is proposed to condition the application to require a BREEAM 'very good' rating is achieved and to ensure 10% of energy generated for the building is from renewable sources. The Sustainability Officer's comments on the proposals are awaited and will be reported direct to Committee.

5.0 CONCLUSION

5.1 The site is located within the built area of the city as defined in the City of York Draft Local Plan. The site has previously been used for industrial purposes in connection with the former use of the site by Nestle/Rowntree and is currently used as a car park. Originally allocated for employment use, the site has not been included as employment land within the employment land review and its retention as employment land is not required. PPS4 says Local Planning Authorities should be supportive of economic development subject to the detailed design of the scheme. There are no policies in the Local Plan that would preclude the use of the site for a residential care home subject to the details of the scheme being acceptable. The principle of the development is therefore considered to be acceptable.

5.2 In Officers' opinion the scheme has evaluated its surroundings and identified a design that has regard to the character and qualities of the area. The building takes reference for its design from its surroundings as required by Government Guidance and Local Plan policies. In terms of landscaping there are no planning policy requirements for minimum garden areas, garden orientation or provision of outdoor facilities for care homes. The landscaping both existing and proposed will provide a good setting for the building and officers are satisfied with the principles of the proposed landscaping scheme. Conditions are proposed to ensure that existing planting is protected during construction and that the landscaping scheme submitted is properly detailed and implemented.

5.3 Highway Network Management raise no objections to the application subject to conditions which ensure the proper implementation of the development, cycle parking provision and travel plan.

5.4 The distance on all boundaries between the building and the adjoining residential properties is considered to ensure the amenity of the properties will not be unduly affected by the development; furthermore all boundaries are reasonably well screened by existing planting and to the east and south the site is separated from the houses by intervening access roads. GP1 of the

Local Plan requires that developments should ensure residents living nearby are not unduly affected by noise, disturbance, overlooking, overshadowing or dominated by overbearing structures. Whilst it is clear that the structures will be visible to adjacent residents and that the overall outlook from the rear of their properties will be changed the details of the scheme are considered to provide an acceptable relationship between existing and proposed development sufficient to accord with the requirements of GP1.

5.5 Additional drainage details are awaited and conditions and further comments will be reported direct to committee.

5.6 The comments of the Sustainability Officer are awaited.

5.7 In all other respects the application is considered to be acceptable subject to appropriate conditions.

COMMITTEE TO VISIT

6.0 RECOMMENDATION: Approve

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans and other submitted details:-

L3066 (9-)902 Rev C
L3066 (- -)21 Rev C
L3066 (- -)22 Rev C
L3066 (- -)13 Rev D
L3066 (- -)20 Rev C
L3066 (9-)904 Rev A
L3066 (- -)25 Rev B
L3066 (- -)23 Rev B
L3066 (- -)12 Rev D
L3066 (- -)11 Rev D
L3066 (- -)10 Rev D
L3066 (9 -)901 Rev B
L3066 (5 -)500 Rev A
11081 - 166 - Topographical survey
2212/3 Revision B

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 VISQ7 Sample panel ext materials to be approv

4 VISQ8 Samples of exterior materials to be app

5 Prior to commencement of the development, an Construction Environmental Management Plan (CEMP) for minimising the creation of noise,

vibration and dust during the demolition, site preparation and construction phases of the development shall be submitted to and approved in writing by the Local Planning Authority. All works on site shall be undertaken in accordance with the approved scheme, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect the amenity of local residents

6 All construction and demolition works and ancillary operations, including deliveries to and despatch from the site shall be confined to the following hours:

Monday to Friday 08.00 to 18.00
Saturday 09.00 to 13.00
Not at all on Sundays and Bank Holidays.

Reason: To protect the amenity of local residents

7 Details of all machinery, plant and equipment to be installed in or located on the development hereby permitted, which is audible at any business, residential or hotel accommodation, shall be submitted to the local planning authority for written approval. These details shall include maximum (L_{Amax}(f)) and average sound levels (L_{Aeq}), octave band noise levels and any proposed noise mitigation measures. All such approved machinery, plant and equipment shall not be used on the site except in accordance with the prior written approval of the local planning authority. The machinery, plant or equipment and any approved noise mitigation measures shall be fully implemented and operational before the proposed use first opens and shall be appropriately maintained thereafter.

Reason: To protect the amenity of local residents and occupants of the care home

8 The building envelope shall be constructed so as to provide sound attenuation against externally generated noise to achieve internal noise levels of not more than 30dB(A) Leq 8 hour from 11.00pm to 7.00am, 45 dB(A) L_{max} between 11.00pm and 7.00am, and 35dB(A) Leq from 7.00am to 11.00pm in all bedrooms of the development, with alternative methods of ventilation where required. The detailed scheme shall be approved in writing by the local planning authority and fully implemented before the development hereby approved is constructed. All works which form part of the scheme shall be completed before any part of the development is occupied. The works provided as part of the approved scheme shall be permanently retained and maintained as such except as may be agreed in writing by the Local Planning Authority. The aforementioned written scheme shall demonstrate that the noise levels specified will be achieved.

Reason: To protect the amenity of future occupants

9 Upon completion of the development, delivery vehicles shall be confined to the following hours

Monday to Saturday	07.00 to 23.00
Sundays and Bank Holidays	9.00 to 18.00

Reason: To protect the amenity of future residents and local neighbours

10 There shall be adequate facilities for the treatment and extraction of cooking odours from kitchens in the care home. Details of the extraction plant or machinery and any filtration system required shall be submitted to the Local Planning Authority for written approval. Once approved it shall be installed and fully operational before the proposed use first opens and shall be appropriately maintained thereafter.

Reason: To protect the amenity of local residents.

11 Prior to the commencement of the development or within such longer period as may be agreed in writing prior to commencement full details of all external lighting including a full Lighting Impact Assessment for all proposals, undertaken by an independent assessor (not the applicant or the lighting provider), shall be submitted to and approved in writing by the Local Planning Authority. The Lighting Impact Assessment shall include the following:

- A description of the proposed lighting: number of lighting columns and their height, and proposed lighting units.
- Drawings showing the illuminance levels (separate drawings for each item listed):
- A plan showing horizontal illuminance levels(Eh), showing all buildings within 100 metres of the site boundary
- A plan showing vertical illuminance levels (Ev), showing all buildings within 100 metres of the site boundary.
- A specification of the Environmental Zone of the application site, as defined in The Institution of Lighting Engineers' Guidance Notes for the Reduction of Light Pollution.
- A statement of the need for floodlighting.

Reason: To protect the amenities of adjacent residents.

12 Unless otherwise agreed by the Local Planning Authority, development other than that required to be carried out as part of an approved scheme of remediation must not commence until parts a to c of this condition have been complied with:

a. Site Characterisation

An investigation and risk assessment, in addition to any assessment provided with the planning application, must be completed in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The contents of the scheme are subject to the approval in writing of the Local Planning Authority. The

investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The written report is subject to the approval in writing of the Local Planning Authority. The report of the findings must include:

- (i) a survey of the extent, scale and nature of contamination (including ground gases, where appropriate);
- (ii) an assessment of the potential risks to:
 - human health,
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - adjoining land,
 - groundwaters and surface waters,
 - ecological systems,
 - archaeological sites and ancient monuments;
- (iii) an appraisal of remedial options, and proposal of the preferred option(s).

This must be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

b. Submission of Remediation Scheme

A detailed remediation scheme to bring the site to a condition suitable for the intended development by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be prepared, and is subject to the approval in writing of the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part IIA of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

c. Implementation of Approved Remediation Scheme

The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report (referred to in PPS23 as a validation report) that demonstrates the effectiveness of the remediation carried out must be produced, and is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

C2 Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of the previous condition, and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with the previous condition.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

13 HWAY40 Dilapidation survey

14 The site shall not be occupied until a Full Travel Plan has been submitted and approved in writing by the Local Planning Authority. The travel plan should be developed and implemented in line with local and national guidelines. The site shall thereafter be occupied in accordance with the aims, measures and outcomes of said Travel Plan.

Within 12 months of occupation of the site a first year travel survey shall have been submitted to and approved in writing by the Local Planning Authority. Results of yearly travel surveys shall then be submitted annually to the authority's travel plan officer for approval.

Reason: To ensure the development complies with advice contained in PPG13 (Transport), and in policy T20 of the City of York deposit Draft Local Plan, and to ensure adequate provision is made for the movement of vehicles, pedestrians, cycles and other forms of transport to and from the site, together with parking on site for these users.

15 Prior to the commencement of any works on the site, a detailed method of works statement identifying the programming and management of site clearance/preparatory and construction works shall be submitted to and approved in writing by the Local Planning Authority. Such a statement shall include at least the following information;

- the routing that will be promoted by the contractors to use main arterial routes and avoid the peak network hours
- where contractors will park
- where materials will be stored within the site
- details of how the car parking area will be managed during the construction period to ensure adequate car parking remains

- measures employed to ensure no mud/detritus is dragged out over the adjacent highway.

Reason: To ensure that the development can be carried out in a manner that will not be to the detriment of amenity of local residents, free flow of traffic or safety of highway users.

16 HWAY21 Internal turning areas to be provided

17 HWAY18 Cycle parking details to be agreed

18 HWAY19 Car and cycle parking laid out

19 No development shall take place until details have been submitted to and approved in writing by the Council of the measures to be provided within the design of the new building and landscaping to enhance the biodiversity of the site. The works shall be completed in accordance with the approved details in accordance with a timescale to be agreed in writing as part of the submitted scheme.

NB. Features suitable for incorporation include measures for species that use buildings such as bats and birds and enhancement of the existing landscape areas that form the boundaries of the site.

Reason: This is proposed to take account of and enhance the habitat and biodiversity of the locality in accordance with PPS9 'Biodiversity and Geological Conservation'.

20 No development shall take place until there has been submitted and approved in writing by the Local Planning Authority a detailed landscape scheme which shall include the species, density (spacing), stock size, and position of trees, and details of ground preparation. This scheme shall be implemented within a period of six months of the completion of the development. Any trees or plants which within a period of five years from the completion of the development, die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of a similar size and species, unless the Local Planning Authority agrees alternatives in writing. This also applies to any existing trees that are shown to be retained within the approved landscape scheme. Any works to existing trees that are protected by a tree preservation order (TPO) or are in a conservation area are subject to local authority approval and notification respectively within and beyond this five year period.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the entire site, since the landscape scheme is integral to the amenity of the development.

21 No development shall take place, including demolition, site clearance and the installation of utilities, until there has been submitted and approved in writing by the Local Planning Authority a method statement regarding protection measures for the existing trees to be retained. This statement shall

include the following: details and locations of protective fencing to BS 5837:2005 to be shown on a plan; phasing of demolition/clearance and landscape works; type of machinery/vehicles to be used (including excavators); parking arrangements for site vehicles; locations for off-loading and storage of materials; location of site cabin.

The protective fencing line shall create exclusion zones that will be adhered to at all times during operations associated with the development. None of the following activities shall take place within the exclusion zones: excavation below existing surface construction, raising of levels, mechanical cultivation, storage of any materials or top soil, lighting of fires, parking or manoeuvring of vehicles, mixing of cement, disposing of washings, storage of fuel. Within the exclusion zone there will be no new trenches or pipe runs for utilities or drains, and no site huts et al. The fencing shall remain secured in position throughout the demolition and construction process. A notice stating 'tree protection zone - do not remove' shall be attached to each section of fencing. All contractor's staff on site shall be made aware of the content of the method statement and its gravity.

Reason: To ensure protection of existing trees before, during and after development which are covered by a Tree Preservation Order and/or make a significant contribution to the amenity of the area and/or development.

22 The development hereby approved shall be constructed to a BREEAM standard of 'very good'. A formal Post Construction assessment by a licensed BREEAM assessor shall be carried out and a copy of the certificate shall be submitted to the Local Planning Authority prior to occupation of the building. Should the development fail to achieve a 'very good' BREEAM rating a report shall be submitted for the written approval of the Local Planning Authority demonstrating what remedial measures shall be undertaken to achieve a 'very good' rating. The remedial measures shall then be undertaken within a timescale to be approved in writing by the Local Planning Authority.

Reason: In the interests of achieving a sustainable development in accordance with the requirements of GP4a of the City of York Development Control Local Plan and the Interim Planning Statement 'Sustainable Design and Construction'

23 No building work shall take place until details have been submitted and approved in writing by the Local Planning Authority to demonstrate how the development will provide, from on-site renewable energy, 10 per cent of the developments predicted energy demand. The development shall be carried out in accordance with the submitted details unless otherwise agreed in writing by the Local Planning Authority. The approved scheme shall be implemented before first occupation of the development. The site thereafter shall be maintained to at least the required level of generation.

Reason: In the interests of achieving a sustainable development in accordance with the requirements of GP4a of the City of York Development Control Local plan and the Interim Planning Statement 'Sustainable Design and Construction'

24 Development shall not begin until a surface water drainage scheme for the site, based on sustainable drainage principles and an assessment of the hydrological and hydrogeological context of the development, has been submitted to and approved in writing by the Local Planning Authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

The scheme shall also include:-

- a minimum of 30% reduction in surface water discharge for any new development from the site
- a design that ensures that storm water resulting from a 1 in 100 year event and surcharging the drainage system can be stored on the site without risk to people or property and without overflowing into the watercourse
- details of how the scheme shall be maintained and managed after completion

Reason: To prevent the increased risk of flooding to improve and protect water quality, improve habitat and ensure future maintenance of the surface water drainage system.

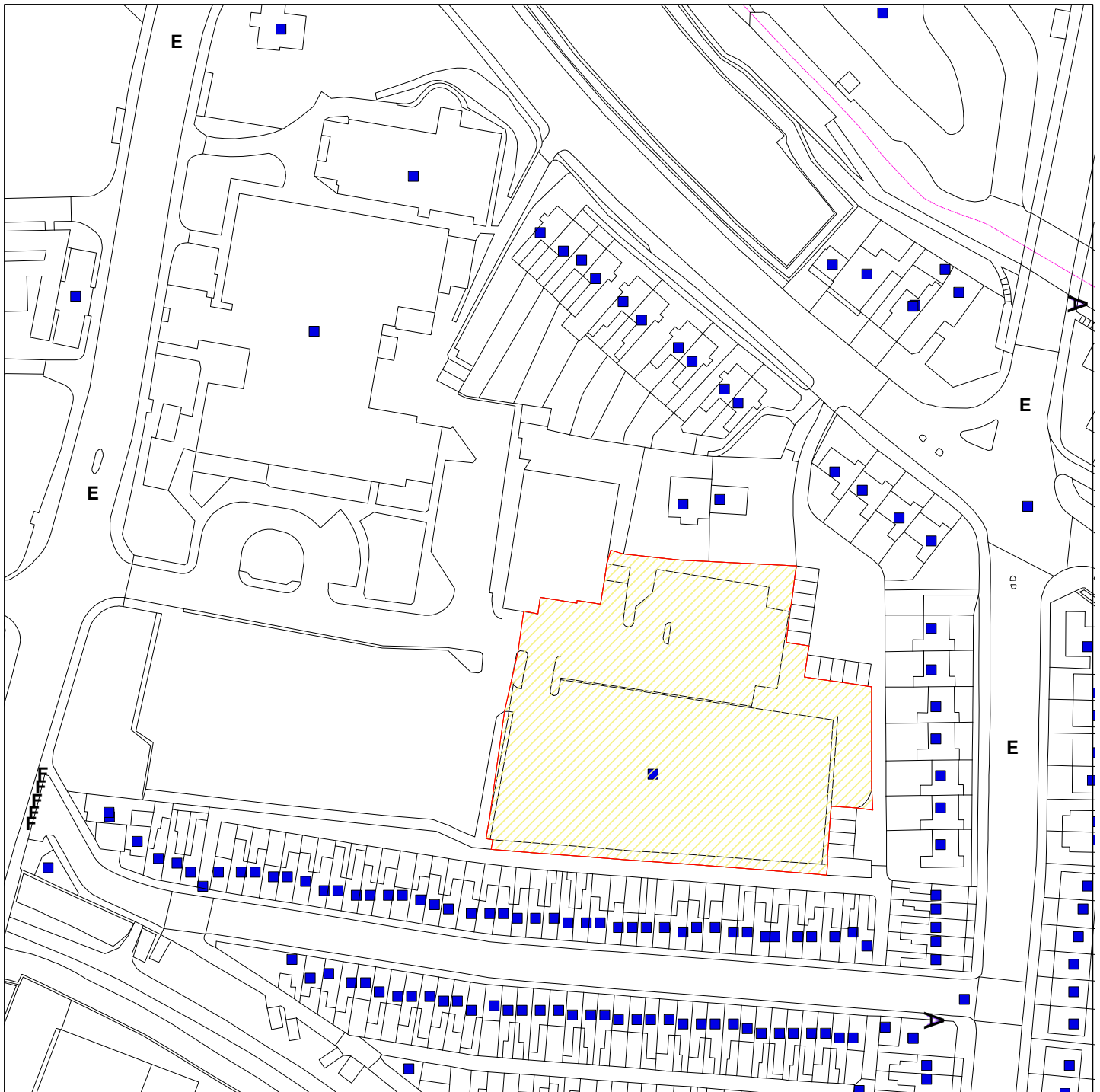
7.0 INFORMATIVES:

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Nuffield Hospital, York

10/01677/FULM



Scale : 1:1250

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Organisation	City of York Council
Department	Planning and Sustainable Development
Comments	
Date	16 November 2010
SLA Number	Not Set

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**PLANS LIST ITEM 4A - UPDATE FOR COMMITTEE, 25 NOVEMBER 2010
NUFFIELD HOSPITAL YORK, HAXBY ROAD, YORK 10/01677/FULM**

Paragraph 4.27 - Drainage raise no objections subject to drainage conditions. Drainage are satisfied with the condition suggested by the Environment agency (condition 24) with slight amendments

Condition 24 amended as follows: -

Development shall not begin until a surface water drainage scheme for the site, based on sustainable drainage principles and an assessment of the hydrological and hydrogeological context of the development, has been submitted to and approved in writing by the Local Planning Authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

The scheme shall also include: -

- A minimum of 30% reduction in surface water discharge for any new development from the site
- A design that ensures that storm water resulting from a 1 in 100 year event and surcharging the drainage system can be stored on the site without risk to people or property and without overflowing into the watercourse. ***Proposed design must also include an additional 20% allowance for climate change.***
- Details of how the scheme shall be maintained and managed after completion
- ***The development should not be raised above the level of the adjacent land, to prevent runoff from the site affecting nearby properties.***

Paragraph 4.28 - The Sustainability Officer is satisfied with the detail of the sustainability information in the application. BREEAM condition and 10% renewables are conditions 22 and 23 on the committee report.

The approved plans condition (Condition number 2) has been amended to update plan numbers

The City Archaeologist confirms that the site lies outside the Area of Archaeological Importance and in an area which has produced evidence for prehistoric activity and stray finds of Romano-British date. The development proposal may have an effect on archaeological deposits which might be preserved within the boundaries of the site. Therefore, an archaeological watching brief

on all ground works for the development will be necessary. Arch 2 is already attached as condition 25 on the agenda.

An additional condition is proposed to ensure the quality of the detail to the heads sill and coping/eaves detail of the roof of the building as follows: -

Large-scale details of the items listed below shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the development and the works shall be carried out in accordance with the approved details.

- Eaves
- Heads, sill and reveals to all window types as appropriate
- Coping to flat roof

Reason: So that the Local Planning Authority may be satisfied with these details.

2.2 Policies:

CYGP4A
Sustainability

CYGP9
Landscaping

CGP15A
Development and Flood Risk

CYGP1
Design

CYT4
Cycle parking standards

CYED9
University of York New Campus

3.0 CONSULTATIONS

INTERNAL

3.1 Highway Network Management - No objections in principle but the access for service vehicles should be revised to remove conflicts with pedestrians.

3.2 Environment, Conservation, Sustainable Development (Urban Design) – No objections to the design and appearance of the building subject to conditions requiring submission of materials and design details. I do have concerns about how the building relates to the other buildings in cluster 2 to the east and the proposed central square to the west. These should be addressed as part of the current masterplanning of cluster 2.

3.3 Environment, Conservation, Sustainable Development (Landscape) - The layout of the building does not encourage drinking/eating activity within the central square. Instead it is directed to what is described within the draft master plan as the 'Academic Square'. Thus there is insufficient clarity as to how the open spaces within this area of the campus will inter-relate and function comprehensively. No objection to the area that is to become the central square being given a temporary landscape treatment until the neighbouring buildings are fully developed. However, it is important to establish in planning terms how implementation of a detailed design for the central square can be secured.

3.4 Structures and Drainage - The site is in flood zone 1 and should not suffer from river flooding. Insufficient information has been provided to determine the impact the proposals may have on existing drainage systems. Nevertheless, now that the central lake is in operation drainage details for the building can be made a condition of approval.

EXTERNAL

3.5 Heslington Forum including Heslington Parish Council - The university gave a brief presentation to the forum on 19 July 2010 prior to submission. No specific comments on the proposals were made by forum members

3.6 Public Consultation - The consultation period expires on 24 November 2010. At the time of writing this report no representations have been received. Any that are subsequently received will be reported to members at the meeting.

4.0 APPRAISAL

4.1 Key Issues:-

- Principle of the Use
- Design and Relationship to the Central Square
- Transport
- Sustainability
- Drainage
- Landscape

RELEVANT LOCAL PLAN POLICIES

4.2 Planning Policy Statement 1 (PPS1) sets out the government's overarching planning policies. It sets out the importance of good design in making places better for people and emphasises that development that is inappropriate in context or fails to take the opportunities available for improving an area should not be accepted.

4.3 PPS25 aims to: ensure that flood risk is taken into account in the planning process; avoid inappropriate development in areas at risk of flooding; and direct development away from areas at highest risk.

4.4 The Draft Local Plan including the 4th set of changes was approved in April 2005. Policy GP1 - Development proposals will be expected to respect or enhance the local environment and be of a density, layout, scale, mass and design that is compatible with neighbouring buildings, spaces and vegetation.

4.5 Policy GP4a - Proposals for all development should have regard to the principles of sustainable development.

4.6 GP9 - Where appropriate, development proposals should incorporate a suitable landscaping scheme that is an integral part of the proposals; includes an appropriate range of species, reflects the character of the area; enhances the attractiveness of key transport corridors; and includes a planting specification where appropriate.

4.7 GP15a - Developers should ensure that the site can be developed, serviced and occupied safely and that discharges from new development should not exceed the capacity of existing/proposed sewers and watercourses.

4.8 ED9 - The scale, layout and design of the new campus at Heslington East should have regard to, inter alia, a comprehensive landscape scheme including publicly accessible public open space and a comprehensive network of pedestrian/cycle routes between campuses.

4.9 T4 - Seeks to promote cycling and states that all new development should provide storage for cycles in accordance with the standards in the local plan.

PRINCIPLE OF THE USE

4.10 The use of the site as part of a new campus was accepted when the Secretary of State granted outline consent in 2007. The use as a social and catering building accords with the land use plan approved as part of the outline consent and the campus masterplan approved in 2008. The building is shown on the draft indicative masterplan for cluster 2, which is currently being discussed with officers.

DESIGN AND RELATIONSHIP TO THE CENTRAL SQUARE

4.11 The draft masterplan for cluster 2 has only recently been submitted to the council. The draft is unacceptable in its present form and discussions between the university and officers are ongoing. Whilst officers would normally expect a masterplan to have been agreed before submission of reserved matters the design of the social and catering building is acceptable. It would be light and airy with a logical layout and crisp external detailing. Materials and design details should be made conditions of approval. The design of future reserved matters applications for cluster 2 will have to take into account the precedent set by the design of the social and catering building.

4.12 The building would face south west thereby addressing movement to and from the hub building across the proposed central square. However, the building's main spill-out space would be to the south east, where the draft masterplan proposes 'Academic Square', a lower-order open space. The central square is an important, central element of the new campus. It was identified as such in the land use plan approved by the Secretary of State and is shown on the approved 2008 masterplan. The draft masterplan for cluster 2 also shows the central square but the indicative arrangement of buildings together with the design of the social and catering building suggest that the central square may not function as envisaged in the outline consent. Clarification and reassurance should be sought from the university as the draft masterplan for cluster 2 develops.

TRANSPORT

4.13 Pedestrian and cycle links will connect the building to the pedestrian ribbon to the north and to the movement spine to the south. The Sheffield stands for 40 bicycles would supplement the bicycle stands provided elsewhere within the new campus. The facilities for access are acceptable subject to resolution of conflict between service vehicles and pedestrians/cyclists. At the time of writing revised access arrangements had only just been received. Members will be updated at the meeting.

SUSTAINABILITY

4.14 The sustainability statement submitted with the application is acceptable. It shows that the building will achieve a BREEAM rating of at least 'very good' and will provide at least 10% of its energy from renewable sources. These should be made conditions of approval.

DRAINAGE

4.15 The site is in flood zone 1 and should not suffer from river flooding. Surface water from the building would be drained by gravity to the lake along the southern side of the site. The water would be stored in the lake and released at agricultural rates into local watercourses. These arrangements, which apply to the campus as a whole, are already approved. Details of the drainage works as they apply to the social and catering building should be made a condition of approval.

LANDSCAPE

4.16 The red line boundary for this application has been drawn fairly tightly around the building but it does include part of the proposed central square. As described above this is the key relationship to the building. The university proposes that the square will not be constructed until the proposed landmark building, on the east side of the square (and close to the social and catering building), has been developed. The rationale for this postponement, which the council accepts in principle, is that the final design of the central square is dependant upon the buildings that front onto it. In the meantime the university proposes a temporary landscape treatment for the central square. Officers have no objection to this subject to the university clarifying how the permanent provision of the central square will be secured.

5.0 CONCLUSION

5.1. The principle of the use of the land for a social and catering building has been agreed and outline planning permission granted. The design of the building is acceptable subject to conditions regarding materials and other details. Nevertheless the relationship of the building to the proposed central square is unclear and should be clarified as part of the masterplanning process.

6.0 RECOMMENDATION: Approve

1 The development hereby permitted shall be carried out only in accordance with the following plans: [Plans list to be tabled at committee].

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

2 Within three months of the commencement of development detailed landscaping proposals for the proposed central square shown on the approved site location plan, together with that part of the central square within the application site shall be submitted and approved in writing by the Local Planning Authority. The scheme shall include the species, density (spacing), and position of trees, shrubs

and other plants; seeding mix, sowing rate, ground levels and swales, hard landscaping materials, lighting, litter/recycling bins and street furniture. Where appropriate reference shall be made to the relevant sections of the Environmental Site Management Plan and/or the Landscape Management Plan. Where required it will also include details of ground preparation. This scheme shall be implemented within a period of six months of the completion of the development. Any trees or plants which within a period of five years from the completion of the development, die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of a similar size and species, unless the Local Planning Authority agrees alternatives in writing.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the entire site, since the landscape scheme is integral to the amenity of the development.

3 Within three months of commencement of development, typical external details at a scale of 1:20 of the following matters (as shown on approved plan 120 Rev E) shall be submitted to, and approved in writing by, the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Main entrance
Colonnade
Roof lantern
Typical bays

Reason: In the interests of the design and external appearance of the building

4 Unless otherwise agreed in writing with the Local Planning Authority, within three months of commencement of development the developer shall submit in writing and be approved by the Local Planning Authority a formal pre-design BREEAM assessment for the design and procurement stages of the development. The developer shall submit a further BREEAM assessment after construction, at a time to be agreed in writing by the Local Planning Authority. The developer shall submit a completion assessment when issued by the BRE. All assessments shall confirm the minimum 'Very Good' rating anticipated in the preliminary BREEAM assessment submitted with the application

Reason: To ensure the development complies with the principles of sustainable development

5 Within three months of the commencement of development full details of the following shall be submitted to and approved by the Local Planning Authority (i) measures to reduce energy demand for the building hereby approved, and (ii) measures to reduce CO2 emissions to a level lower than required under Building Regulations Part L. The development shall be carried out in accordance with the approved details.

Reason: To ensure the development complies with the principles of sustainable development.

6 No building work shall take place until details have been submitted to and approved in writing by the Local Planning Authority, to demonstrate how the applicant will provide, from renewable sources, 10% of the building's total energy demand on land within the control of the applicant. The development shall not be occupied until these works have been carried out in accordance with the submitted details unless otherwise agreed in writing by the Local Planning Authority (as part of an energy strategy for the Heslington East campus). Not later than 12 months after the building has first been brought into use, the applicant shall submit an Energy Statement to the Local Planning Authority, which details the percentage of the buildings energy consumption that has been derived from renewable sources. Thereafter the Energy Statement shall be submitted on an annual basis unless otherwise agreed in writing with the Local Planning Authority.

Reason: In the interests of sustainable development

7 Notwithstanding any proposed materials specified on the approved drawings or in the application form submitted with the application, samples of the external materials to be used shall be submitted to and approved in writing by the Local Planning Authority within three months of commencement of the development. The development shall be carried out using the approved materials.

Reason: So as to achieve a visually cohesive appearance.

8 The building shall not be occupied until the cycle parking areas and means of enclosure shown on the approved plans have been provided within the site. These areas shall not be used for any purpose other than the parking of cycles.

Reason: To promote use of cycles thereby reducing congestion on the adjacent roads and in the interests of the amenity of neighbours.

9 Within one month of commencement of development, details of foul and surface water drainage works shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with these approved details.

Reason: So that the Local Planning Authority may be satisfied with these details for the proper drainage of the site and that provision has been made to maintain the existing drainage in accordance with policy GP15a of the Development Control Local Plan and PPS25 (Development and Flood Risk).

10 No mechanical, electrical, telecommunications or other plant, equipment or apparatus (other than as shown on the approved plans) shall be installed on the roof of the building without the prior written consent of the Local Planning Authority).

Reason: In the interests of the design and external appearance of the building

7.0 INFORMATIVES:

Notes to Applicant

1. In the opinion of the Local Planning Authority the proposal, subject to the conditions listed above, would not cause undue harm to interests of acknowledged importance, with particular reference to the principle of the use, scale and appearance of the building, landscaping, transport, sustainability and drainage and constructional impact. As such the proposal complies with Policies GP4a, GP9, GP15A, ED9 and T4 of the City of York Local Plan Deposit Draft.

Contact details:

Author: Kevin O'Connell Development Management Officer

Tel No: 01904 552830

Revisions	
Rev No	Date
A	24.09.10
SITE ACCESS AMENDED BIKE STORE AMENDED ADDITIONAL BIKE STAN ADDED	
B	08.11.10
Site Layout ammended	

PLANNING

July 1991

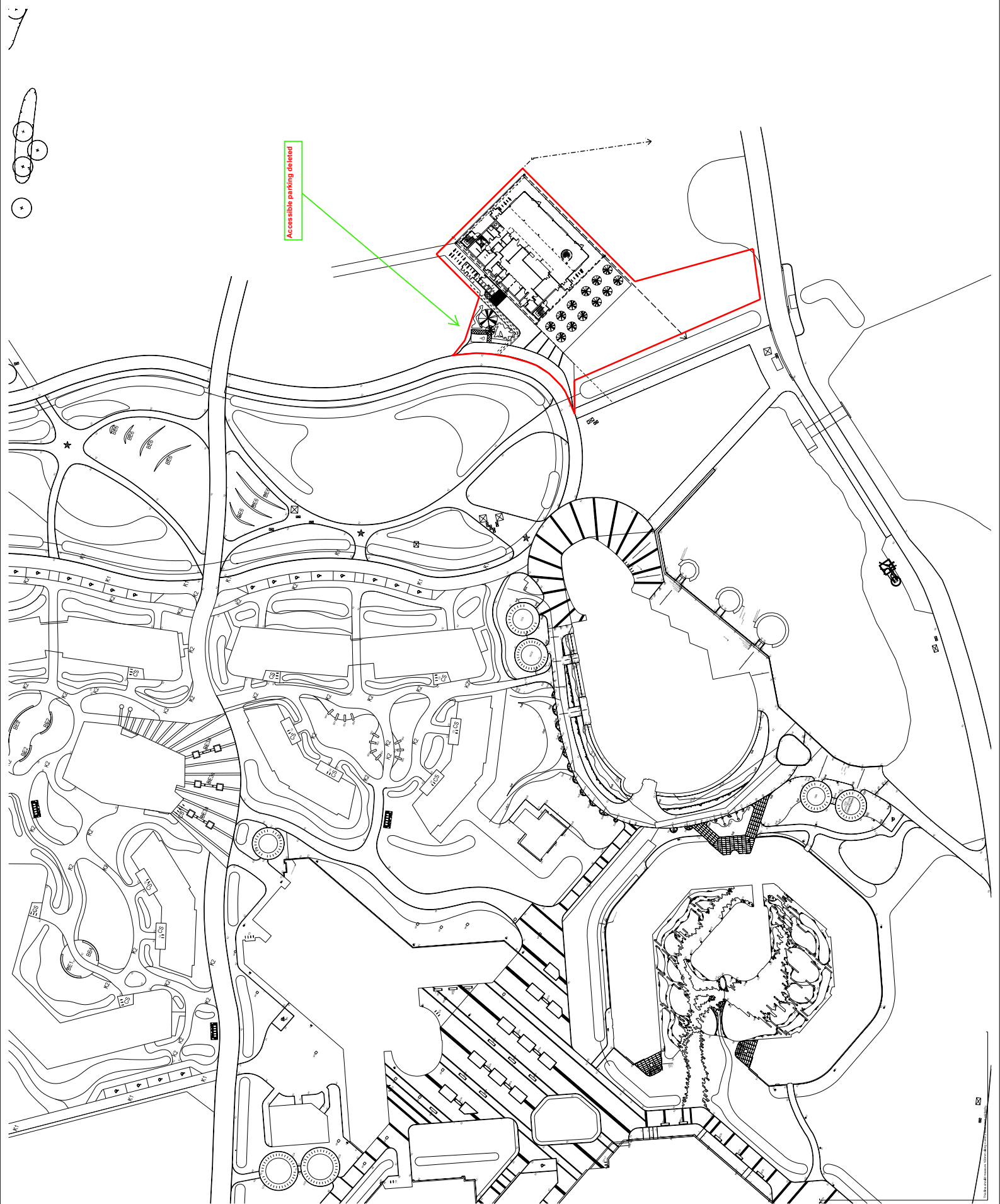
University of York

Social and Catering Building

Drawn by E.R.

Campus Site Plan

Check all dimensions and verify on site. Report any errors or omissions	
18.05.10	
Drawn by	AF
Checked by	A1
Drawn by	1:500
Reviewed by	RO
Job title	UOY1
Drawn by	002
Checked by	B
Project	



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**East Area Planning Sub-Committee Update
25 November 2010**

**10/02140/REM
Social & Catering Building, Heslington East Campus, York**

Revised Conditions

1. The development hereby permitted shall be carried out only in accordance with the following plans: 001/J received 24 November 2010 and 100/G, 101/E, 102/C, 120/E, 121 and 110/D received 5 October 2010.

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

The second is Condition 11, an additional condition, requiring the submission of details of the vehicular access for service vehicles and the proposed disabled parking space. Discussions between highway colleagues and the university are not yet complete, hence the need for the condition.

11. Within one month of commencement of development, details of the access, parking and manoeuvring arrangements for service vehicles and of the disabled persons parking space(s) shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with these approved details.

Reason: In the interests of highway safety.

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UNIVERSITY OF YORK HESLINGTON EAST CAMPUS: CUMULATIVE DEVELOPED FOOTPRINT

Application	Application Reference	Footprint (m ²)	Constructed
Car Parks and Access Roads			
Interchange Car Park and Access Road	08/02043/REMM	7278	Yes
Northern Service Road	08/02323/REMM	3165	Yes
Western Service Road	09/00808/REM	841	Yes
Movement Spine (within allocated area)	09/01241/REM	4640	Yes
TOTAL for car parks and access roads:		15,924	
Buildings: Cluster One			
Goodricke College	08/00032/REMM	5500	Yes
Substations (N & S)	09/00008/REM	214	Yes
	09/00012/REM		
TFTV	08/01751/REMM	3012	Yes
Computer Science	08/02446/REMM	2856	Yes
Law and Management	08/02573/REMM	2692	Yes
Hub building	08/02572/REMM	2600	Yes
Foul pumping station	08/02689/FUL	0	Yes
Catalyst building	09/01472/REMM	1021	Yes
TFTV ancillary buildings and Robot Lab	09/01376/REM	1014	Yes
	10/00245/REM	211	
TOTAL for Cluster One:		19,120	
Buildings: Cluster Two			
Energy Centre	10/01825/REMM	1348	No
Social and Catering Building	10/02140/REMM	903	No
TOTAL		37,295 (3.7ha)	
Cumulative footprint as percentage of allocated area		5.73%	

Uhesc1011.tab.gh

November 2010

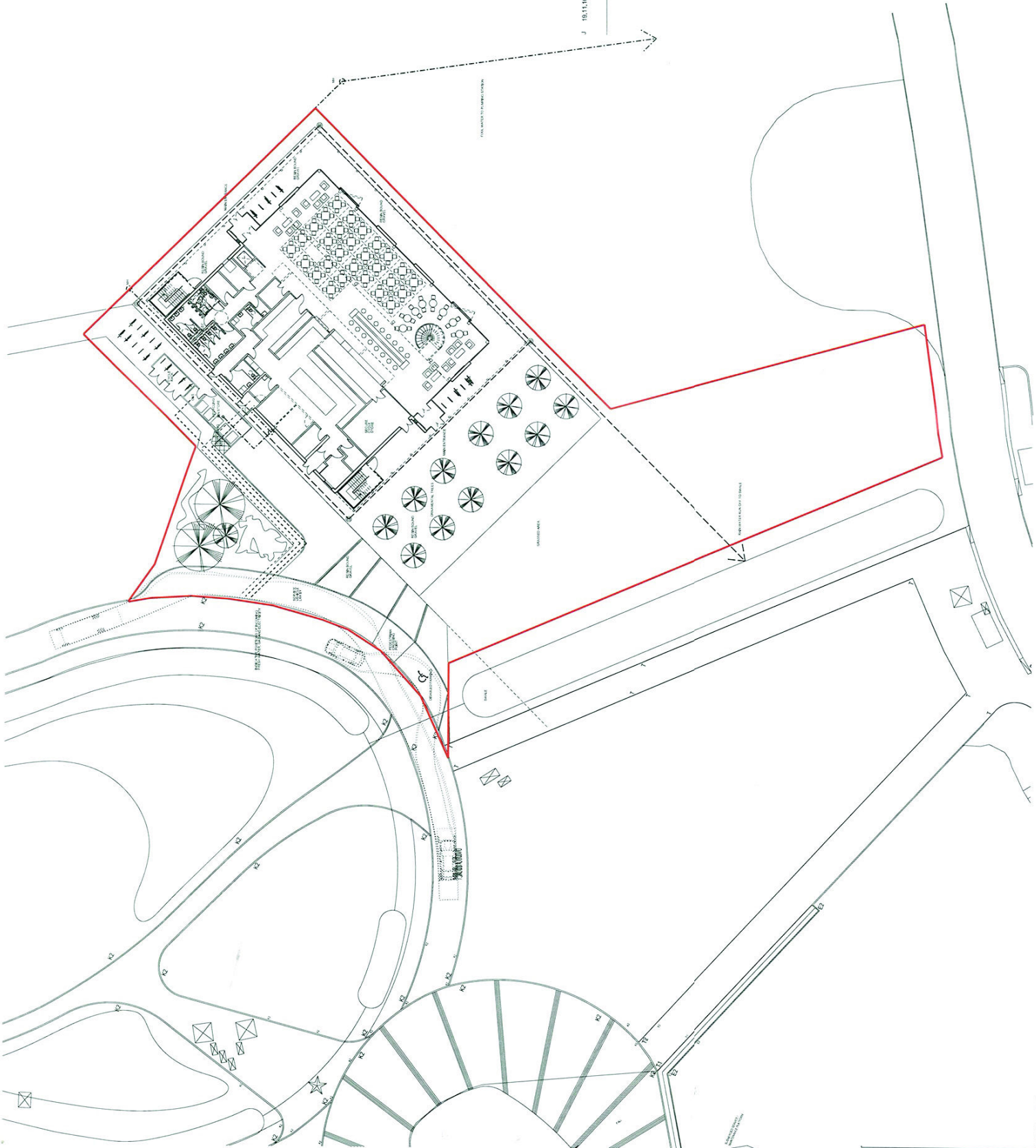




INDICATIVE ONLY

Cluster 2 Masterplan in Campus Context

DRAFT FOR COMMENT



North Arrow

J 19.11.10 BOLLARDS REMOVED, AF
REMOVED SERVICE DROP
OFF AMENDED.

RO A 24.05.10 INDICATIVE LINES OF
RECYCLING AREA ADDED
CONCRETE AREA ADDED
SUPPLY DOWN AREA
ADDED AND ENTRANCE AND
TURNING HEAD REALIGNED

B 20.07.10 INDICATIVE LINES OF
RECYCLING AREA ADDED
LANDSCAPING REMOVED

C 26.07.10 RED LINE BOUNDARY
AMENDED

D 12.08.10 PLANNING

E 31.08.10 BICYCLE RACKS ADDED AF RO

F 24.09.10 SITE ACCESS AMENDED AF RO
DISABLED CAR PARKING
ADDED
ADDITIONAL BICYCLE PARKING
ADDED

G 26.09.10 LANDSCAPING AMENDED AF RO

H 08.11.10 BUILDING LOCATION A AF RO
HANDSTANDING AMENDED

PLANNING

University of York
Social and Catering Building

Site Plan

Check all dimensions and verify on site. Report any errors or omissions

14.05.10	AF
AF	A1
1:200	RO
UOY1	D
001	J

14.05.10
AF
A1
1:200
RO
UOY1
D
001
J



External view of East corner of Building



Planning Committee**25 November 2010**

Report of the Assistant Director (Planning and Sustainable Development)

THREE CONSERVATION AREA APPRAISALS FOR STRENSALL, STRENSALL RAILWAY BUILDINGS AND TOWTHORPE VILLAGE CONSERVATION AREAS: CONSULTATION DRAFTS**Summary**

1. This report requests that three separate draft Conservation Area Character Appraisals for the neighbouring conservation areas of Strensall (No.23, 19.16ha), Strensall Railway Buildings (No.31, 1.58ha) and Towthorpe Village (No.32, 8.81ha) are approved for public consultation. Copies of the appraisals are in **Annex A**.

Background

2. Strensall Conservation Area was originally designated in 1979 and was extended in 2001 following public consultation. Also in 2001, Strensall Railway Buildings and Towthorpe were designated as two new conservation areas following the request of Strensall and Towthorpe Parish Council. These conservation areas are the subject of 3no separate draft appraisal documents. Prior to the production of the draft appraisals the character of these three conservation areas had been summarised in conservation statements within Appendix B of the City of York Draft Local Plan.
3. Both Strensall and Towthorpe Village conservation areas were identified as “conservation areas at risk” by English Heritage (EH) in 2009 (ref. English Heritage’s ‘Heritage At Risk’ information www.english-heritage.org.uk). At Towthorpe the significant number of redundant farm buildings was cited as being of particular concern; whereas at Strensall general development pressures were identified as leading to some loss of special character. Based on the findings of EH’s survey, the City of York Council prioritised these conservation areas in their rolling programme of producing Conservation Area Appraisals for each of their 35 conservation areas. Given the proximity of Strensall Railway Buildings within the Parish, it seemed logical to review this conservation area at the same time, as appraisal work and consultations could run simultaneously saving both time and resources. The work is being jointly funded by Strensall and Towthorpe Parish Council and from the existing budget of the Design Conservation and Sustainable Development Section.
4. The appraisals offer an opportunity to re-assess the character of the designated

areas and the extent of their boundaries and to evaluate and record their special interest. New guidance on Planning for the Historic Environment, PPS 5, advises local authorities to develop a firm evidence base for plan-making (policy HE 2) and to use established methods of environmental appraisal to better understand the significance of heritage assets such as conservation areas. By defining the special architectural and historic interest which makes designation justified, development control decisions will be better informed. The appraisal should also provide a basis for making sustainable decisions about the future of an area through the development of management proposals.

5. The draft conservation area appraisals are attached at **Annex A1, A2 and A3**. The documents have been developed by a team of independent conservation consultants. The format and content of the report have been discussed with officers in the Design Conservation and Sustainable Development Section of the City Strategy Directorate and there has been an interim meeting with the Parish Council.
6. If adopted the appraisals would form part of the review process incumbent on Local Authorities under section 69 of the Planning (Listed Buildings and Conservation Areas) Act. An important element of this process is a reassessment of the conservation area boundary.
7. Each appraisal follows the sequence recommended by English Heritage. The first sections (2-4) describe the context and development of the areas and draw attention to factors which have contributed to their present day character. Section 5 outlines the findings of the boundary reviews. Section 6 describes the general character of the areas (as revised). In the case of Strensall four different character areas have been identified and each of these is analysed in more detail in sections 7-10. The final section of each appraisal outlines issues which should be incorporated into future management strategies.

Boundary Reviews

8. **Strensall** - Chapter 5 proposes two areas of extension to the conservation area boundary (areas B&C) as shown on the map in **Annex B1**. It is proposed that Areas B and C be added due to their historical significance to Strensall. Area B is to the north of Southfields Road, which is an important historic back lane defining the rear boundaries of the properties along the main street, and Area C includes some impressive properties associated with the railway along Princess Road and Moor Lane. A further area was put forward for exclusion from the conservation area (area A) due to its modern layout and appearance. For reasons related to the historical significance of the block plan it has been decided to retain the current boundary position.
9. **Strensall Railway Buildings** – Chapter 5 recommends that the boundary be extended to include the storage facilities associated with the former railway station (Area A, map **Annex B2**), and to the west the boundary would extend around numbers 93-103 The Village. The latter buildings appear contemporary with other houses in the area which were developed around the turn of the century (C19th-20th) to take advantage of the new railway halt (Area B, map **Annex B2**).

10. **Towthorpe Village** – Chapter 2 concludes that no areas are considered to be included inappropriately within the current boundary and no further areas are proposed to be added (map **Annex B3**).

Consultation

11. This report seeks approval for a consultation process to take place. If Members agree the draft appraisals, then a public consultation will be carried out based upon the methods used for other conservation areas. This would take place over a six week period in December and January as follows:

- A letter to consultees including Conservation Area Advisory Panel, the Local Ward Members, Strensall and Towthorpe Parish Council, Strensall Local History Group and appropriate amenity bodies (see list of consultees in **Annex C**).
- A single leaflet summarising the three appraisals and boundary changes, with a tear-off comments slip, will be distributed to all properties within and adjoining the proposed conservation area (approximately 400 households as opposed to the whole parish which would be approximately 2372 households). The map in **Annex D** shows the existing Conservation Area boundaries, proposed amendments, and suggested delivery area for the leaflet. The Parish Council have kindly offered to assist with the delivery of the leaflet.
- Copies of the appraisal documents will be made available at the Council offices 9 St Leonard's Place, Strensall Parish Office, Strensall Library, The Half Moon Public House and The Ship Inn as well as on the Council and Strensall and Towthorpe Parish Council websites.
- Representatives of the Consultants and the City of York Council will attend Strensall and Towthorpe Parish Council meeting on 11th January 2011 to allow residents to see the documents and talk to the team who will be on hand to help with any queries. We propose to run an exhibition/ drop-in session before the start of the meeting, rather than hold a separate event, and consider that this along with the delivery area of the leaflet will provide sufficient coverage for the Appraisal.
- A press release will be sent to the York Press, and posters advertising the exhibition and meeting on the 11th January and also where the appraisals can be viewed will be placed in local venues in order to promote it to the wider village residents. An article will also feature in the December/ January parish 'Outreach' magazine which is delivered to each property in Strensall and Towthorpe.

Options

12. Option 1 – approve the draft document(s) for consultation purposes and approve the method and range of consultation.
13. Option 2 – amend the draft document(s) and/or change the method and range of the consultation process.
14. Option 3 – do not approve the draft document(s) for consultation.

Analysis

15. Unless any obvious omissions or inaccuracies can be identified within the draft documents, or concerning the consultation process proposed, option 1 would allow the consultation process to start in November. Subsequent feedback from this process could be assessed comprehensively rather than in stages as implied by option 2. Option 3 would halt the statutory process and this should only be considered in exceptional circumstances.

Corporate Priorities

16. The appraisals aim to assist in improving the actual and perceived condition and appearance of each of the conservation areas. This aim supports the following objectives of the corporate strategy: “thriving city”, “inclusive city” “sustainable city”. It would also provide a more informed basis for decision making and for policy formulation.
17. The appraisal would contribute towards the Local Authority’s statutory obligations with regard to conservation areas in their control.

Implications

18. **Financial** – Production of the consultation documents and delivery costs would be met by existing budgets and from a significant contribution by the Parish Council.
19. **Human Resources (HR)** – No known implications.
20. **Equalities** – Different formats of the consultation leaflet will be made available on request. Documents will be lodged in accessible locations.
21. **Legal** – No implications at this stage.
22. **Crime and Disorder** – No known implications.
23. **Information Technology (IT)** – No implications.
24. **Property** – No implications.

Risk Management

25. There should be no additional risks.

Recommendations

26. i) Members are asked to agree Option 1 to approve the 3no draft Conservation Area character appraisals (Strensall, Strensall Railway Buildings and Towthorpe Village), as proposed in Annex A1, A2 & A3, for use as consultation documents.

- ii) Members are also asked to approve the proposed consultation methods.

Reasons:

- (1) the documents have been prepared in accordance with current guidance from English Heritage. They have adopted a structured approach to the assessment process and as consultation documents they are clearly written and capable of being amended where required
- (2) the boundary reviews have been carried out in accordance with the Planning (Listed Buildings and Conservation Areas) Act 1990 and conform with the latest policy guidance on the historic environment - PPS 5 and the related practice guide.
- (3) the proposed consultation process would be based on previous practice

Contact Details

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Chief Officer Responsible for the report:

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Assistant Director (Planning & Sustainable Development)

Katherine Atkinson
Community Planner
Tel 01904 551694

Report
Approved



Date 12 November 2010

Design Conservation and
Sustainable Development

Specialist Implications Officer(s) none

Wards Affected – Strensall

All

For further information please contact the author of the report

Background Papers:

PPS 5 Planning for the Historic Environment - March 2010
PPS 5 Historic Environment Planning Practice Guide - March 2010
Guidance on conservation area appraisals - English Heritage 2006

Annexes

- Annex A1 - Strensall Conservation Area Appraisal consultation draft
- Annex A2 - Strensall Railway Buildings Conservation Area Appraisal consultation draft
- Annex A3 - Towthorpe Conservation Area Appraisal consultation draft
- Annex B1 - Map showing the existing boundary and proposed amendments to Strensall Conservation Area
- Annex B2 - Map showing the existing boundary and proposed amendments to Strensall Railway Buildings Conservation Area
- Annex B3 - Map showing the existing boundary of Towthorpe Conservation Area
- Annex C - List of consultees
- Annex D – Map showing the proposed consultation area

STRENSALL CONSERVATION AREA APPRAISAL



In association with Strensall with Towthorpe
Parish Council

CONTENTS

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3 Topography and Setting	4
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7 Church Lane	12
8 Manor Farm	14
9 The Village	16
10 Princess Road/Moor Lane	19
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APPENDIX A - Listed Building Descriptions	

This document was prepared in November 2010 by Woodhall Planning and Conservation on behalf of the City of York Council. The format and content were agreed with the Design, Conservation and Sustainable Development Section of City of York Council, and the document was approved by the Council on

I INTRODUCTION

I.01 A conservation area is defined by the Planning (Listed Buildings and Conservation Areas) Act 1990 as an area of special architectural or historic interest, (the character or appearance of which) is desirable to preserve or enhance. Section 69 of the Act requires Local Planning Authorities to identify and designate conservation areas within their geographical boundaries and to formulate, in conjunction with the public, proposals for their preservation and enhancement (Sections 71 and 72 of the Act). The Act places a duty upon Local Planning Authorities to review the extent of the conservation areas.

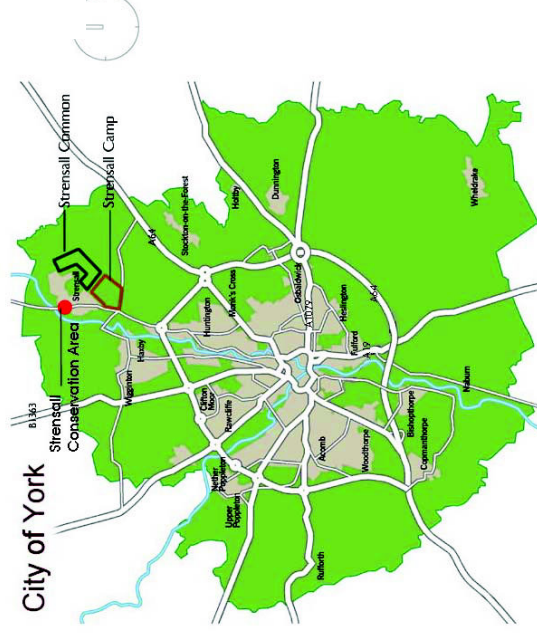
I.02 Strensall is a historic village, located to the north of York (see Map 1). The Strensall Conservation Area was designated in 1979 and was extended following public consultation in 2001 to include riverside pasture land and paddocks to the north-west, buildings along West End and south along Princess Road see Map 2. In addition, a small area on the eastern edge of the village is designated as the Strensall Railway Buildings Conservation Area and this is the subject of a separate Appraisal document.

I.03 This report contains an assessment of the Conservation Area and is in four sections. The first part (Chapters 1-4) sets the scene by analysing the history and baseline factors of the conservation area. The second part (Chapter 5) reviews the extent of the Conservation Area as designated and explains the reasons for the recent revision of its boundaries. The third part describes the area as revised, firstly in general terms (Chapter 6) and then, in more detail, as four sub-areas (Chapters 7-10). The final part (Chapter 11) discusses the measures that could be introduced within the designated area in order to preserve and enhance its character and appearance.

I.04 The assessment is based on field work in August and September 2010 and is limited to the buildings and areas visible from the public domain.

I.05 Strensall Conservation Area Appraisal was approved as a supporting document to the conservation policies included in the City of York's draft Local Plan (incorporating the fourth set of changes) by the Planning Committee of City of York Council *to be inserted*. The Conservation Area Appraisal will also be

used to inform the emerging Local Development Framework.



Map 1

2 LOCATION AND CONTEXT

2.01 The Strensall Conservation Area lies approximately 5 kilometres north of York city centre. The core of the village is on the south side of the River Foss and is positioned between the B1363 and A64 (see Map 1). The Strensall Conservation Area covers the heart of the historic settlement to the north of the railway line (see Plate 1).

2.02 The designated area is focused around the historic core of the village and includes St Mary's Church to west, farmland up to the River Foss to the north and the properties on both sides of the main road, The Village, as far as Northfields to the east.



Plate 1 Aerial view

3 TOPOGRAPHY AND LANDSCAPE SETTING

3.01 Strensall village occupies part of a flat open area to the south of the River Foss. The village itself is surrounded by fields to the north, east and west. These areas of open ground enable long distance views towards the village from the north, in particular towards the church and Manor Farm which are set on slightly higher ground. To the south of the village is Strensall Common and Strensall Army Camp.

3.02 The immediate setting of the designated area is dominated by twentieth-century housing development to south, west and east, though tree fringes help to mitigate their effect. To the north lies open ground used for agricultural purposes (see Plate 2). Although the immediate setting, adjacent to Strensall Bridge, is compromised by the partially demolished former tannery building. As a result the Conservation Area is predominantly enclosed by surrounding development with only the open ground to the north providing relief to this enclosure.



Plate 2 View west out of the designated area from Strensall New Bridge

4 HISTORICAL DEVELOPMENT

4.01 The name Strensall probably indicates an Anglo-Saxon settlement, although there may have been activity in or around the site of the village at earlier periods. Pre-historic and Roman remains have been found in the area and it may have always been a convenient crossing point of the River Foss. It has been suggested that the village was the site of the Synod of Whitby, which may indicate that it was also an early monastic site.

4.02 Strensall was included in the Domesday Survey of 1086, as one of the estates of the Archbishop of York, and it was described as; “Strenshale, Sasford and Turchil held five geldable carucates of St Peter. There is land for two ploughs, it is waste.” At this early period Strensall appears to have been within the forest of Galtres that stretched north of York. Throughout the thirteenth century the prebendaries of Strensall (one of the canons of St. Peter’s, York) developed their own hunting preserve on the edge of the royal forest.

4.03 Manor Farm (formerly known as Strensall Hall) occupies a moated site which was probably the administrative centre of the village from the early medieval period (and possibly earlier).

The land close to the hall was known as Coney Garths and may be the site of the free warren granted to Gerard de Vuippens in 1292 (‘coney’ being another name for rabbit). Other evidence for the early history of the settlement includes the presence of ridge and furrow (signs of medieval ploughing) around the village.

4.04 Strensall has a typical medieval pattern of properties, with narrow-fronted plots of land that extend back on either side of a single west-east street (the present Church Lane and The Village - see Plates 3 & 4). The plots on the north side were bounded by the River Foss and those on the south side stretched to Back Lane (now Southfields Road). Other roads led out of the village to the north and south. The church was at the west end of the village, close to the hall. The medieval fields lay on both sides of the River Foss, with common land (Strensall Common) to the south of the village. The inhabitants of the village also had grazing rights within the forest.

4.05 The hearth tax returns for 1663 suggest that there were around 46 houses within the village at that date, with a likely population of approximately 200. The enclosure of the medieval fields during



Plate 3 View east along The Village



Plate 4 View east along The Village



Plate 5 St Mary's Church

the seventeenth and eighteenth centuries and a corresponding improvement in agriculture led to a slow increase in the size of the village, which by 1801 had a population of 297. Unusually, the enclosed fields seem to have retained the form something of the long narrow strips that are characteristic of farming patterns within medieval fields. Also, Strensall Common remained open until the late nineteenth century.

4.06 A school was founded in Strensall in 1718-19, probably on the site of the nineteenth century building that has recently been converted into residential accommodation. The church was completely rebuilt between 1801 and 1803, only to be replaced again in 1865-6 (see Plate 5).

4.07 The Foss Navigation Company was formed in 1793 with the intention of forming a navigable link between the junction of the Rivers Foss and Ouse in central York north to Stillington Mill. During 1796 and 1797 two locks and a bridge (Strensall New Bridge - See Plate 6) were constructed at Strensall as part of this work. During the first five years of the nineteenth century a canal was dug from north of Strensall to Sheriff Hutton Bridge but work then stopped. Strensall Bridge, designed by the eminent York architect John Carr, was constructed in

1798, replacing an earlier bridge.

4.08 The eighteenth and early nineteenth centuries appear to have been a time of prosperity for the village as many of the older houses date from this period. Also, a number of new religious buildings were erected, including a Quaker meeting house, and two Methodist chapels (one Primitive, the other Wesleyan-See Plate 7). This period may also have seen the establishment of the tannery on the north side of the river, next to the bridge.

4.09 The opening of the York to Scarborough railway in 1845, which ran to the south of the village, led to a rapid reduction in the use of the canal. By 1854 the majority of The Village, Church Lane and West End had been developed as can be seen from the first Ordnance Survey map. Only the vicarage and its outbuilding existed on the south side of York Road. Interestingly, there were few buildings on the land to the south of the church (identified as Butts End), possibly because it was glebe land and lay between the church and the vicarage. Also, no development had taken place on Moor Lane (the northern section of which is now known as Princess Road), although the map evidence indicates the existence of a gatekeepers cottage (now demolished) at the level crossing.



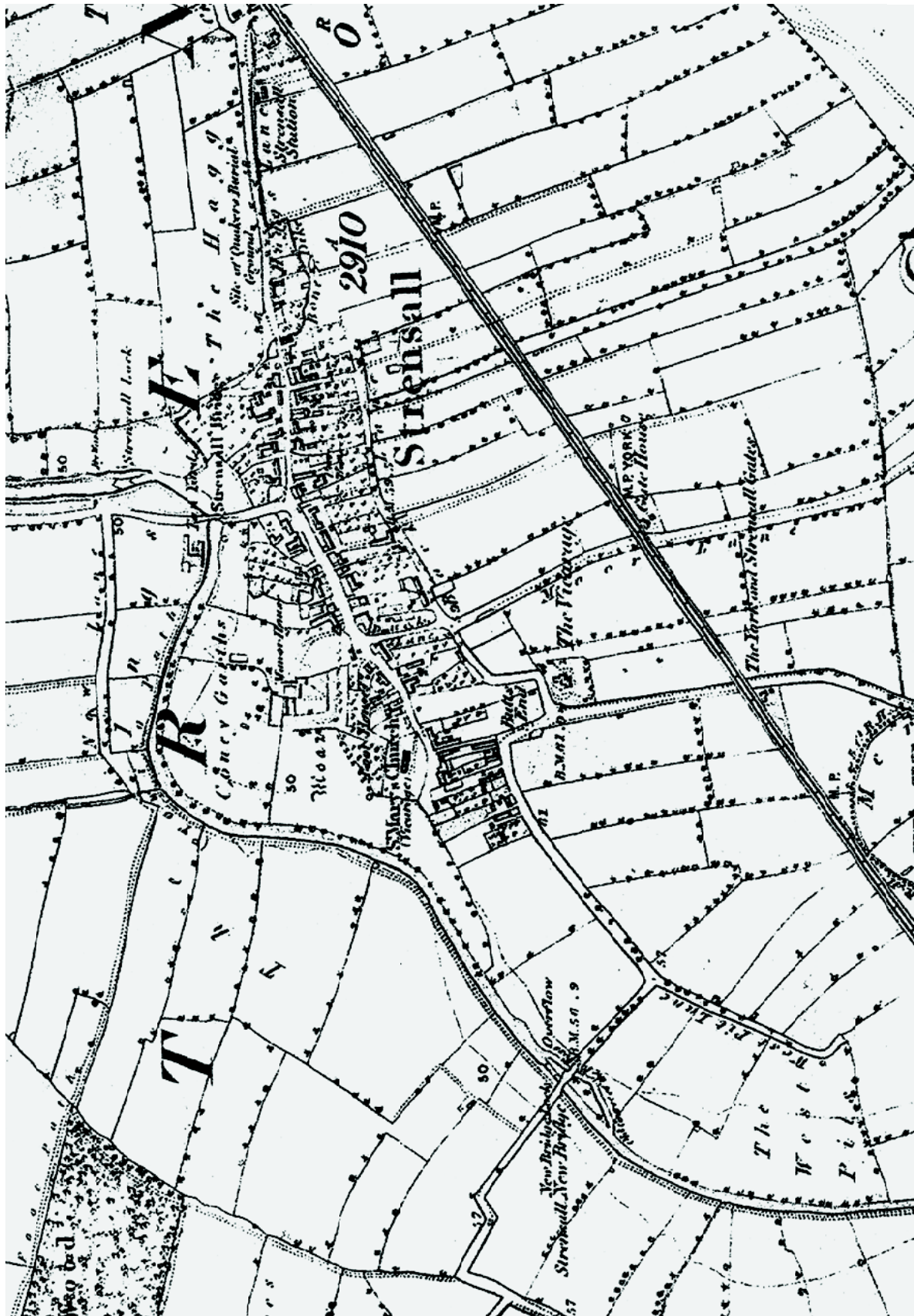
Plate 6 Strensall New Bridge



Plate 7 Former Chapel, Church Lane



Plate 8 The Poplars from Princess Road



1852 Ordnance Survey Map

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1911 Ordnance Survey Map

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4.10 Strensall did not expand significantly during the second half of the nineteenth century, although as noted in 5.06 above the church was rebuilt in 1865-6. A house (now The Poplars - see Plate 8) was built on the south side of York Road at its junction with Princess Road and in 1895 a new Wesleyan Chapel (the present Methodist Church) was constructed (see Plate 9).

4.11 By early twentieth century little had changed in the area. Duck Lane appears to have been improved to provide better access to the York Road and new houses began to appear on the east side of Moor Lane (now partly Princess Road - see Plate 10). Further housing was also constructed along West End and the school was expanded. At the east end of The Village a fine single storey house named Meadowside (No. 69) was built.

4.12 By 1930 No. 2 West End (named Oakleigh at that date) had been built. Also, the Ordnance Survey map of that date notes the existence of Strensall Halt at the level crossing on Moor Lane. Twenty years later there had been much more development along Moor Lane and by the early 1970s there had been extensive infill development throughout the village. This included building on the north side of Southfields Road (formerly Back Lane) and the construction of St.

Mary's Close and Church View (now West End Close) on the open space to the south of the church. Also, there had been considerable development to the south and west of the historic core.

4.13 By the end of the twentieth century houses had also been built in the grounds of No 69 The Village and infill development has also occurred to the north of The Grange. The beginning of the twenty-first century saw a rapid increase in the number infill developments. Examples include new housing to the south of Yew Tree Farm and Hawthorn Mews on the north side of The Village (see Plate 11).



Plate 9 Methodist Church, The Village



Plate 10 Mid twentieth century housing on Princess Road



Plate 11 Hawthorn Mews

5 CONSERVATION AREA BOUNDARY

5.01 One of the requirements of an appraisal is a review of the Conservation Area boundaries to establish whether changes would be appropriate if it might be appropriate for there to be any changes. Therefore the extent of the existing designated area has been reviewed and areas around the existing Conservation Area have been studied to ascertain whether any new areas should be included or existing areas excluded.

Existing boundary

5.02 The current boundary of the designated area is illustrated on Map 2 in Appendix A. To the north the existing boundary follows the line of the River Foss (see Plate 12) from Strensall New Bridge east to Strensall Bridge and Bone Dike. It then follows the line of Bone Dike, wrapping around the north-east corner of the village, excluding Netherwoods, and running south down the west side of Northfields to connect to The Village.

5.03 The boundary continues south across The Village and includes the Sycamore Farm complex. It then runs west along Bone Dike for a short distance before turning south to include Yew Tree Farm. The boundary cuts through Badger Cottage and runs west, following the rear

(southern) boundaries of the properties that front The Village, before it turns south down the rear (eastern) boundaries of the houses along Princess Road.

5.04 The boundary continues to the terraced houses before returning north up the western side of Princess Road along the back edge of the footpath. The designated area includes The Poplars, on the junction with York Road, and the Old Vicarage, on the south side of York Road opposite its junction with West End.

5.05 The boundary crosses York Road (see Plate 13) and runs north to include the houses on the east and north sides of West End. It then turns north, excluding the Primary School from the designated area, to connect back to Strensall New Bridge.

Possible alterations

5.06 A small section of the Conservation Area has been considered for exclusion within the designated area due to its lack of special architectural interest. This Area is annotated as Area A on Map 2 in Appendix A and includes St Mary's Close off the south side of Church Lane (see Plate 14), Nos. 5-11 Church Lane, West



Plate 12 View looking east along the River Foss



Plate 13 View north along York Road



Plate 14 St Mary's Close

End Close (see Plate 15), Nos. 4-8 West End (east side) and Nos. 8-16 on the north side of York Road. This is an area of late twentieth century houses and bungalows which includes two small cul-de-sacs. However, the “off set” branch in the road alignment from York Road suggests the site has historic significance worthy of protection. The road from York focuses directly towards the church spire across the site. In addition, the atypical pattern of the development does not disrupt the streetscene as it is internal to the area. The gardens offer mature hedging and 3 no. large trees as amenity to the wider environment including the east boundary of the alley to Church Lane. On balance it has been decided to retain this area within the Conservation Area.

5.07 It is recommended that two areas are included within the Conservation Area. These are annotated as Areas B & C on Map 2.

5.08 Area B is on the north side of Southfields Road, an important historic lane, defining the original rear (southern) boundaries of the properties along the main street. It is therefore considered that, although few of the buildings in this area are of special

interest, the historic significance of the road justifies its inclusion within the designated area (see Plate 16).

5.09 Area C is located at the southern end of the Conservation Area. Princess Road and Moor Lane form part of the historic road pattern of the village and the houses represent the limited expansion of the area following the arrival of the railway. In particular, Nos. 5-11 Moor Lane are very good examples of late nineteenth/early twentieth century large villa pairs (see Plate 17). It is also understood from historic map records that a station halt and gate house were located at the level crossing, which is almost certainly one of the reasons why impressive properties were developed in this area. This halt was used by troops from the nearby Strensall Army Camp during World War Two.

Recommendations

5.10 In view of all the above, it is considered that Area A should remain within the designated area and Areas B & C be included due to their historical significance within Strensall.



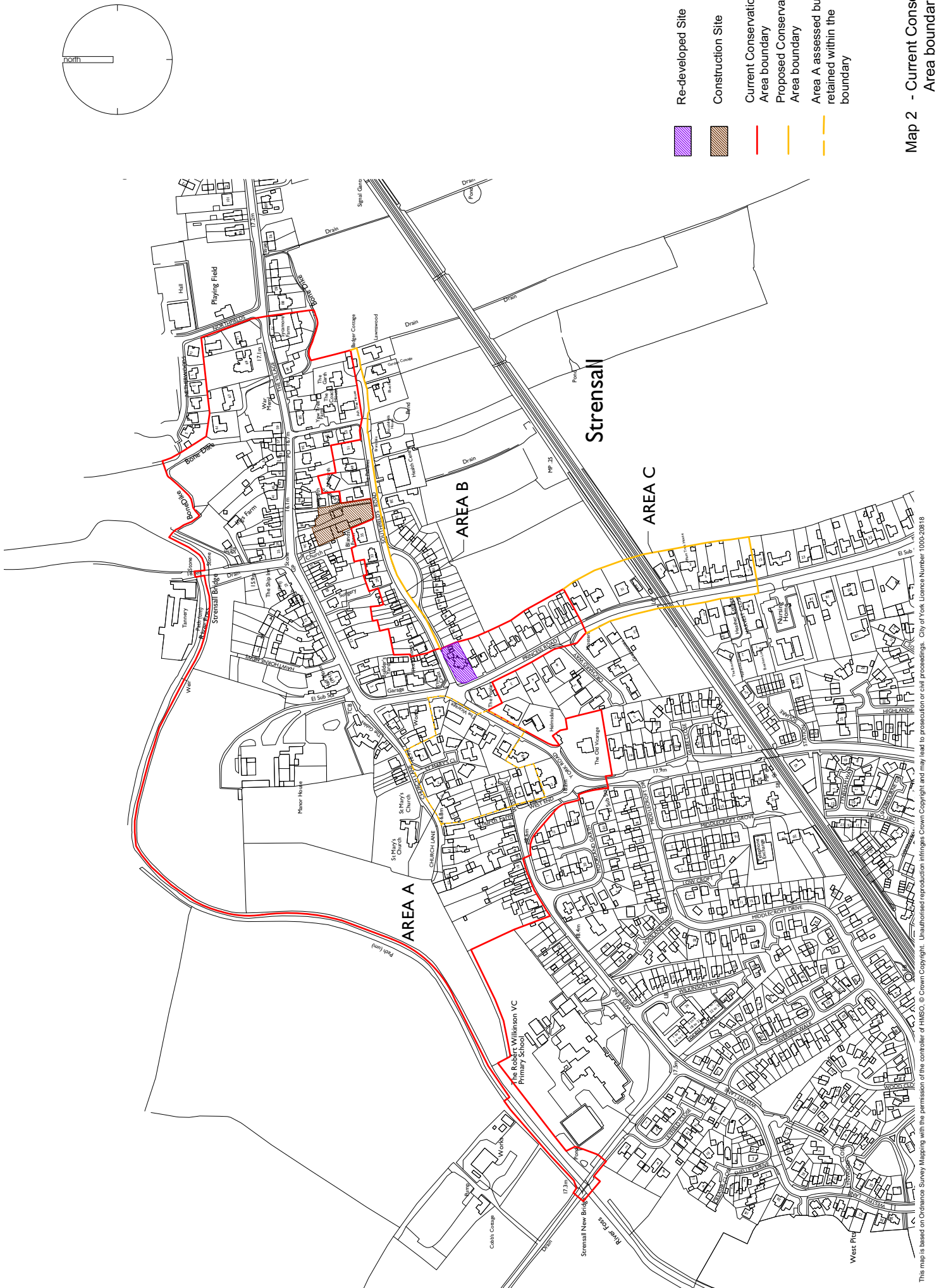
Plate 15 West End Close



Plate 16 View west along Southfields Road



Plate 17 Nos. 9 & 11 Moor Lane



6 GENERAL CHARACTER

- 6.01 This section considers the general character of the whole of the Conservation Area, as distinct from the analysis of the four character areas that have been identified (see 6.09 below).
- 6.02 The key characteristic of the Conservation Area is that of a linear village that, unusually, has no through route at its western end. The later development to the south and west of the historic core results in some loss of the village character but the river and the open ground to the north retain the rural setting on this side of the Conservation Area.
- 6.03 The form of the village is principally established by the long narrow plots on either side of the main west-east roads, with buildings fronting the roads and many outbuildings in the long rear gardens (see Plates 18 and 19). These rear gardens provide relief to the enclosed village streets. However, there are many instances where gardens have been developed for housing, which has to an extent eroded the historic pattern of development in the area.
- 6.04 The roads allow medium distance views within the designated area and there are



Plate 18 View of West End



Plate 19 The Village



Plate 20 Village shops

also limited views into the designated area towards the church tower which is a local landmark. Views from the north across the open fields to the church are of particular importance.

6.05 The village itself is a busy rural settlement with a number of shops and local services. This activity gives the village vitality and enables the designated area to retain its village character (see Plate 20).

6.06 Within the designated area there are no public open spaces. The only open areas are the grounds of St Mary's Church (see Plate 21) and the open ground and wooded areas adjacent to the river.

6.07 The many mature trees within the village, in particular around the church, the northern boundary of the village envelope and along The Village itself (particularly at its east end) soften the visual appearance of the area (see Plate 22).

6.08 The Strensall Conservation Area includes seven buildings or structures that are included on the List of Buildings of Special Architectural or Historic Interest (including the two bridges across the river - see Plates 6 & 23). Listed building descriptions are located in Appendix A.

6.09 The character of the Conservation Area is fairly consistent and the village is generally perceived as one entity. However, the four areas identified below (and shown on Map 3) do have some different characteristics and are therefore analysed separately in the following sections:

- Church Lane
- Manor Farm
- The Village
- Princess Road/Moor Lane



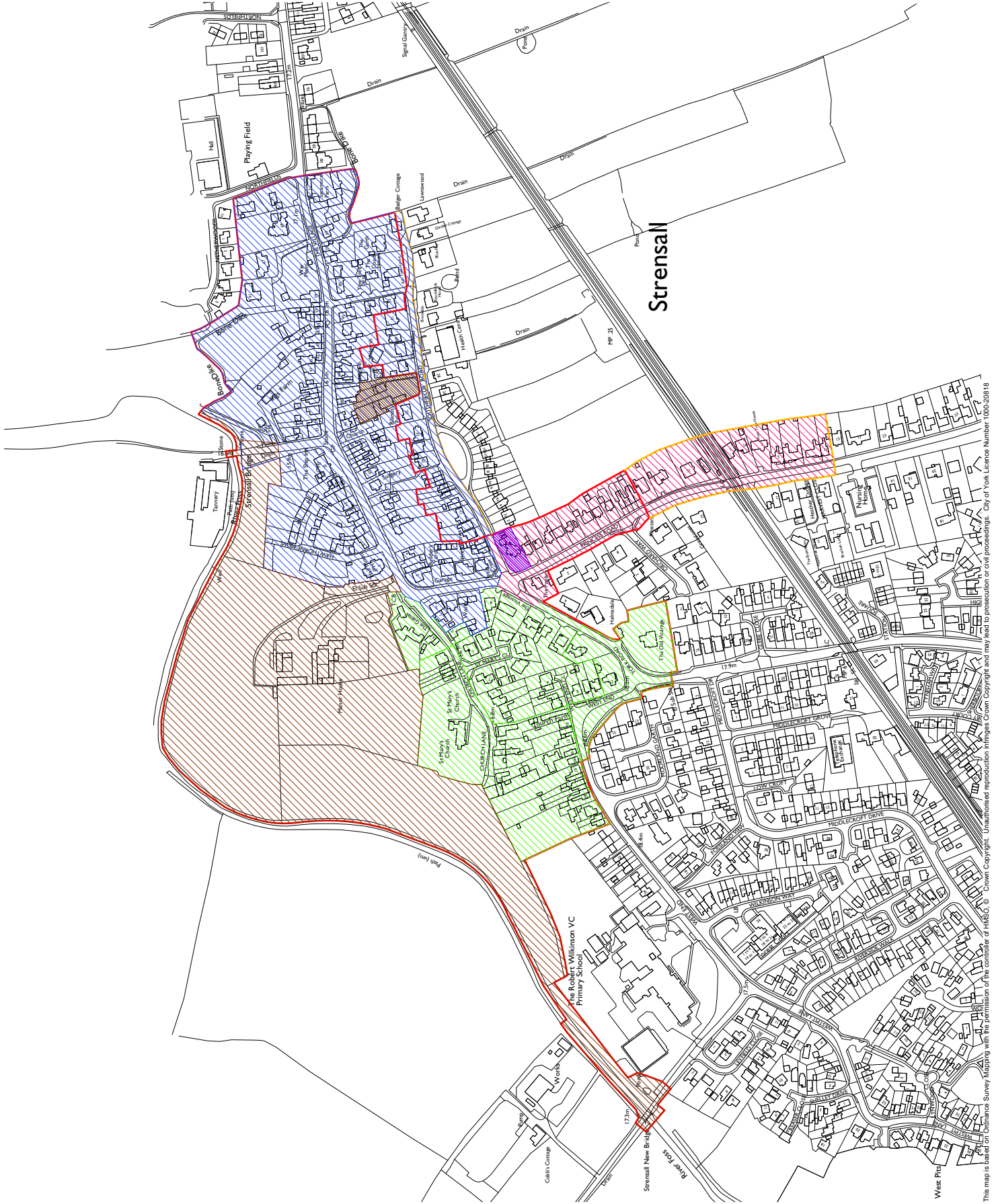
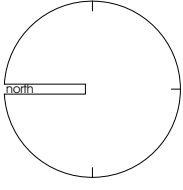
Plate 21 Grounds of the church



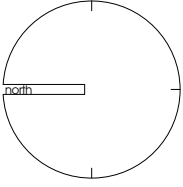
Plate 22 Trees along The Village



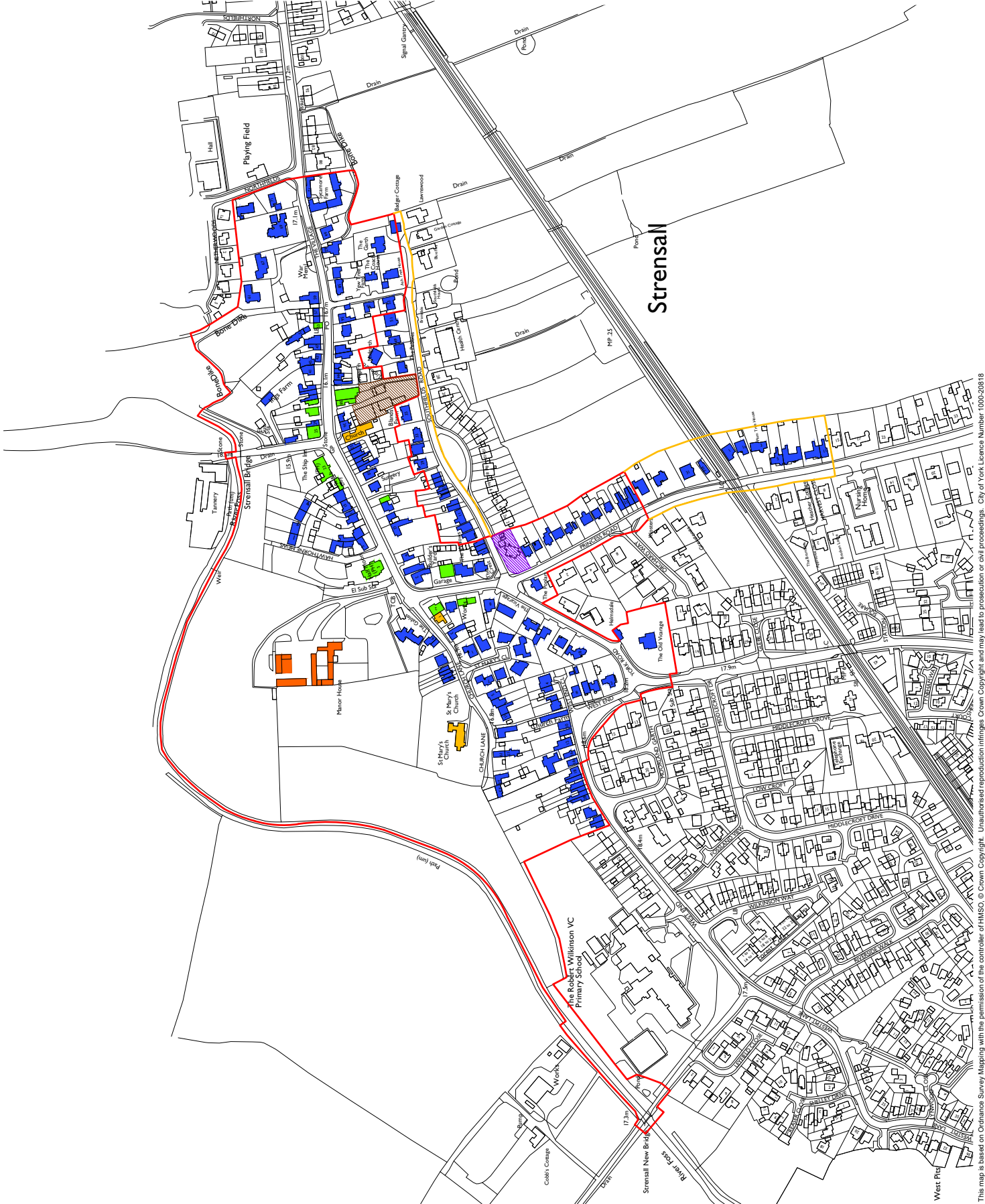
Plate 23 Strensall New Bridge

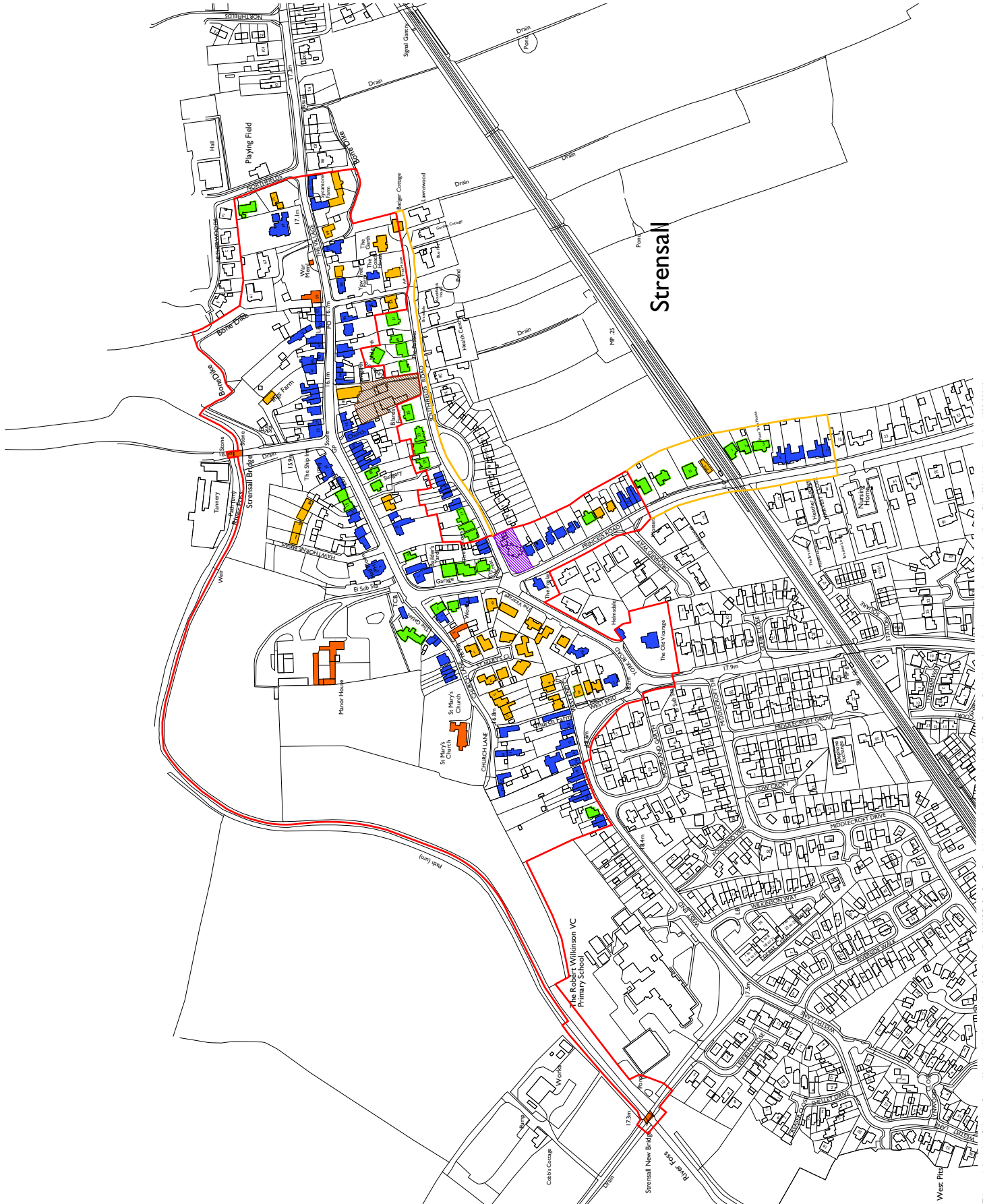


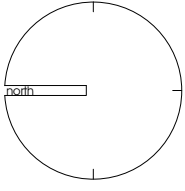
- Current Conservation Area boundary
- Proposed Conservation Area boundary
- Church Lane
- Manor Farm
- The Village
- Princess Road
- Re-developed Site
- Construction Site



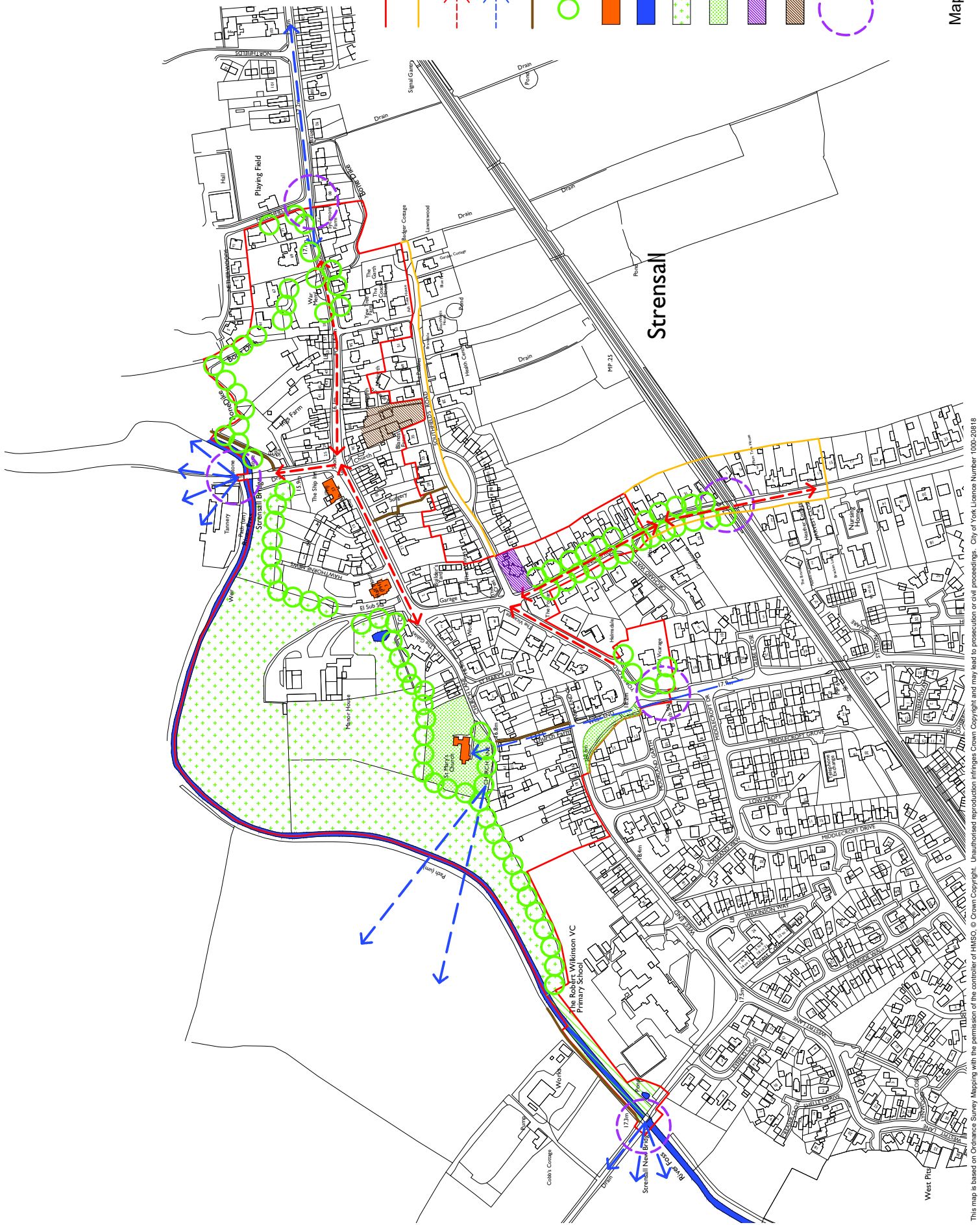
- Current Conserv Area boundary
- Proposed Conserve Area boundary
- Residential
- Commercial
- Community
- Religious
- Education (all levels)
- Agricultural and Horticultural
- Re-developed Site
- Construction Site







- Current Conservation Area boundary
- Proposed Conservation Area boundary
- Medium distance
- Long distance vi
- Public Footpaths
- Trees
- Landmarks
- Water
- Agriculture
- Grassed Area
- Re-developed Site
- Construction Site
- Gateway



7 CHURCH LANE

- 7.01 This character area consists of Church Lane and parts of West End and York Road (see Map 3).
- 7.02 The special interest of this character area is the result of a number of different factors: the tight enclosure of Church Lane and the visual dominance of the church; the varied relationship of buildings to York Road; and the architectural quality of the buildings.

Uses

- 7.03 The buildings in this character area are all residential with the exception of St Mary's Church (see Map 4).

Qualities of buildings

- 7.04 Within this character area there are two listed buildings (see Appendix A): St Mary's Church (see Plate 24); and No. 3 Church Lane - Berwin House, which is a mid-eighteenth century house (see Plate 25). Most of the buildings are of a modest two storey height. They are generally of simple form, with vertical window openings, constructed in clamped fired, red brick, with tiled or slated pitched roofs. The church is very different, having a complex form with a prominent tower and spire.

- 7.05 Local detailing includes brick cambered or flat arch lintels, and decorative banding at eaves level. Some of the earliest buildings also have triangular brick panels - known as brick tumbling on their gables. Most retain their original features, such as sash windows and cast iron downpipes. Also, within this character area are a number of larger or more prominent detached properties which reflect the expansion of the village during the late nineteenth and early twentieth centuries. Examples include the Vicarage and No. 2 West End (see Plate 26). All the historic buildings, listed or not, contribute to the character and the appearance of the area (see Map 5).

Spaces and views

- 7.06 The open space within this character area is limited to the churchyard which is set behind a tree filter along its boundary with Church Lane. This churchyard consists of maintained grass and memorial stones.
- 7.07 There is a significant view north along West End towards the church tower (see Plate 27). No. 2 West End as a minor landmark building on York Road, particularly in views looking north. Some



Plate 24 St Mary's Church



Plate 25 No. 3 Church Lane (Berwin House)



Plate 26 No. 2 West End

limited views also occur along Church Lane and West End in both directions.

7.08 There is an important pedestrian route within this character area. This is the narrow pathway, enclosed by high walls that connects Church Lane with West End. It is an important historic route within the village and due to the enclosure provided by the garden walls and high beech hedging of the adjacent properties the footpath retains its historic interest and significance despite being finished with tarmac (see Plate 28).

Soft landscaping

7.09 There are a large number of mature trees within this section of the Conservation Area (see Map 6). Many are located in the grounds of the church. Other trees are located in the gardens of the various houses. The Vicarage on York Road, which has very significant trees along its boundary that prevent public views towards the building. The numerous trees and hedges make a vital and varied contribution to the character of this area.

Neutral and negative factors

7.10 Where recent developments have a modest form and make use of appropriate building materials and detailing, they can be considered as neutral factors within the area (Map 5).

7.11 Within the character area are a number of modern detached modern houses that do not have architectural interest or historic characteristics that contribute to the character of the area (see Plate 29 and Map 5).



Plate 27 View towards Church Tower



Plate 28 View of alley



Plate 29 Large modern house on Church Lane

8 MANOR FARM

8.01 This character area is focused around Manor Farm (previously named Strensall Hall) which was the administrative centre of the medieval village, together with the open areas that extend north to the River Foss. It extends from Strensall New Bridge in the west to Strensall Bridge to the east (see Map 3).

8.02 The special interest of this area is centred on the historic complex of buildings and land at Manor Farm, the medieval moated site and its open setting.

Uses

8.03 Within the area there is only one property, Manor House and its associated agricultural buildings (see Map 4). The remainder of the character area is made up of open farmland and wooded areas adjacent to the river.

Qualities of buildings

8.04 Manor House is a Grade II listed building (see Appendix A). It dates from the late seventeenth century but was substantially altered in the eighteenth and twentieth centuries (see Plate 30 and Map 5).



Plate 30 View towards Manor Farm



Plate 31 Strensall New Bridge



Plate 32 Remnants of lock entrance

Other Structures

8.05 Within the character area there are two listed bridges (see Appendix A). Strensall New Bridge is located at the western end of the designated area and is the only remaining bridge built by the Foss Navigation Company in the late eighteenth century (see Plate 31). On the east side of the bridge are the remains of associated walls which could be related to former locks (see Plate 32). These remnants add to the interest of this particular part of the character area. Strensall Bridge, which dates from 1798, carries the main road north from the village (see Plate 33).

Spaces and views

8.06 The open spaces around Manor Farm define this particular character area. The trees at the western end of the area and along the rear boundaries of the village envelope create a distinctive rural setting (see Plate 34). Views into the designated area, and to the church tower, from across the River Foss are important, particularly those that are possible along the public footpath that follows the northern bank of the river (see Map 6).

8.07 A panoramic view looking north out across open fields is possible from Strensall Bridge and serves as a reminder of the agricultural history of the village.

8.08 The wooded area on the south side of the river close to Strensall New Bridge contains an informal footpath used for recreational purposes (see Plate 35).

Soft landscaping

8.09 The majority of this character area consists of grassland used for agricultural purposes and the wooded area on the south side of the river adjacent to Strensall New Bridge (see Plate 35 and Map 6). The boundary of the village envelope also contains significant trees.

Neutral and negative factors

8.10 There are no neutral or negative factors within this character area.



Plate 33 Strensall Bridge



Plate 34 View into the area across the River Foss



Plate 35 Wooded area and informal footpath to the south of Strensall New Bridge

9 THE VILLAGE

9.01 This character area (see Map 3) consists of the main road through the village which runs in a east/west direction and contains properties on both sides of the road. It also includes the houses on the north side of Southfields Road, which it is proposed should be included within the Conservation Area (see Chapter 5.00).

9.02 The special interest of this character area is the result of a number of different factors: the architectural quality of the buildings; the historic plot layouts; the relative variety of the buildings in terms of style, size; and their relationship to the road.

Uses

9.03 The buildings accommodate a mixture of residential and commercial uses typically found in a village. Facilities include two public houses, a garage, the local library, a post office, a convenience store, the Methodist Church and the fish & chip shop (see Map 4).

Qualities of buildings

9.04 This character area includes two listed buildings (see Map 5 and Appendix A): No. 59 The Village a late eighteenth century house, remodeled in the early nineteenth century (see Plate 36) and the war memorial (see 9.07 below). Most buildings are of a modest two storey height and form part of small collections of buildings or terraces. They are



Plate 36 No. 59 The Village



Plate 37 Gable detailing



Plate 38 The Ship Public House

of simple form, with vertical window openings, constructed in clamp-fired, red brick, with tiled or slated clear roof slopes interrupted only by the numerous chimneys. The more formal buildings in the area are set behind plinth walls and railings.

9.05 Local detailing includes brick cambered or flat arch lintels, and decorative banding at eaves level. Some of the earliest buildings also have triangular brick panels - known as brick tumbling on their gables (see Plate 37). Many of the eighteenth and nineteenth century buildings retain original features, such as sash windows and cast iron downpipes. Some of the buildings within the area show evidence of adaptation of the fabric, such as the former Butchers which has been increased in height. Most of the historic buildings, whether listed or not, contribute to the character and the appearance of the area (see Map 5). Some of the buildings within this character area have rendered or painted facades, for example The Ship Public House (see Plate 38). Although the use of render and painted brickwork (see Plate 39) is not the predominant material for external walls within the area, these buildings also contribute to the character and appearance of the area.

9.06 Most of the more modern buildings in this character area are constructed in a sympathetic brick and are of a simple form.

However, there are several examples of houses that do not conform to the local vernacular and many of these do not follow the fine urban grain of the designated area.

Other structures

9.07 The very fine war memorial is a Grade II listed structure (see Appendix A) that is located on the north side of The Village. It consists of small hard landscaped area with the memorial raised on a series of steps. The tall back-cloth of hedging and shrubs provides a suitable back-drop to the Portland stone (see Plate 40).

Spaces and views

9.08 The spaces in this character area are predominantly limited to the streetscene as most of the open areas are the rear gardens of the various houses, which cannot generally be seen from the public domain. However, there are number of examples of building enclaves set around a shared yard area. The only public space is the small landscaped area of the war memorial (see Plate 40).

9.09 There is a designated public footpath that runs from The Village, adjacent to No. 18, through to Southfields next to No. 29. This is a tarmac footpath and has no historic features. A second, undesignated, pedestrian route runs adjacent to the convenience store on The Village



Plate 39 No. 69 The Village



Plate 40 The War Memorial



Plate 41 View south along unpaved route

through to Southfields. This is a wide route and has an unmade surface but appears to be of historic significance (see Plate 41). Other secondary tracks have been tarmaced which erodes part of the character of the area.

9.10 Medium distance views exist along The Village in both directions but are limited by the curvature and undulations of the road but enables unfolding views along the main road (see Map 6).

9.11 A long distance view out of the designated area is possible from the eastern end of The Village. Similarly, views into the Conservation Area are also possible, looking west along the section of The Village that is outside the designated area.

9.12 There is a short distance view over the hump of Strensall Bridge looking south towards the Methodist Chapel on the south side of the Village. This building closes the vista and acts as a local landmark (see Plate 42).

Soft landscaping

9.13 There are a large number of mature trees and hedges within this character area. Most of the trees and hedges are located within the curtilage of the various buildings but they make a very important contribution to the streetscene (see Plate 43 and Map 6).

9.14 Many of the houses on the north side of The

Village have long rear gardens, some of which have been infilled with new housing. However, to the east of Strensall Bridge a number of the houses retain their long rear gardens, which adds to sense of immediacy to the surrounding countryside in this part of the area.

Neutral and negative factors

9.15 Where recent developments within this character area have a modest form and massing that responds to historical precedents within the village, and make use of appropriate building materials and detailing, they can be considered as neutral factors within the area (see Map 5).

9.16 However, there are a number of examples of buildings that do not have architectural interest or historic characteristics that contribute to the character of the area, such as the typical late twentieth century bungalows on The Village (see Map 5 and Plate 44).

9.17 As a result of the commercial uses within the area a number of the retail units have inappropriate shopfronts. The introduction of large plastic fascias is considered to be a negative factor (see Plate 20). The extent of illuminated signage also creates a harsh effect on the village ambience at night.



Plate 42 View looking south from Strensall Bridge



Plate 43 Trees in the streetscene



Plate 44 View of bungalows The Village

10 PRINCESS ROAD/MOOR LANE

10.01 This character area is focused around the historic route of Princess Lane and Moor Lane (which was formerly all known as Moor Lane) at the southern end of the designated area. It includes housing on the east side of the highway only (see Map 3).

10.02 The special interest of this character area is the result of its development during the late nineteenth and early twentieth century. As a result, this area has a distinct suburban character, very different from the traditional village qualities of the rest of the Conservation Area.

Uses

10.03 The buildings in this character area are all in residential use (see Map 4).

Qualities of buildings

10.04 There are no listed buildings within this character area but there are several buildings of note (see Map 5).

10.05 Nos. 19-25 Princess Road form a short terraced block, two and a half storey in height, built in the late nineteenth century (see Plate 45). As result the buildings are more elaborate in appearance than those elsewhere in the

Conservation Area. They make use of contrasting brickwork and decorative timber eaves. The form of the buildings is also different, with gables in the roof, canted bay windows on the ground floor and bracketed porches over the front entrance door.

10.06 Nos. 5-11 Moor Lane are excellent examples of large, early twentieth century, semi-detached suburban houses (see Plate 46). These two pairs represent housing built as a result of the railway and are important both historically and architecturally to the village. The massing of these buildings is very bold, with each house having a prominent gable that are is paired with its neighbour. This form coupled with the use of render and Venetian-style windows at second floor level provide a very individual appearance. Other details are more restrained but at ground floor level there are canted bay windows linked by a canopy that also extends over the entrance doors.

10.07 Other buildings of interest include the inter-war housing at the northern end of Princess Road. These buildings appear to have been built in close succession and follow a similar building form. They



Plate 45 Nos. 19-25 Princess Road



Plate 46 View of 9 & 11 Moor Lane



Plate 47 View of inter-war housing

are small detached houses, two storeys in height but with the first floor partially contained in the roof and with only one central small window at first floor level facing the street. The use of a central roof gable and gable roof porch adds interest to these buildings (see Plate 47).

Spaces and views

10.06 The public space within this character area is limited to the streetscape. However, the rear gardens to the various properties are also of importance, although they cannot generally be seen from the public domain.

10.07 Views within the character area limited to those north and south along the highways (see Plate 48). Views into the Conservation Area are possible from Moor Lane looking north and the trees (outside the designated area) that are seen in the equivalent view south are of significance. Views out of the character area are also possible from the level crossing along the railway tracks in both directions (see Plate 49).

Soft landscaping

10.08 The grass verge down the west side of Princess Road and Moor Lane is important to the streetscene as it softens the hard landscape and includes several mature trees, which contribute to the

leafy appearance of this character area (see Map 6). In addition, many of the front gardens of the housing on the east side of the highway have mature trees which also contribute to the character of this area.

Neutral and negative factors

10.09 There are a number of examples of buildings that do not have architectural interest or historic characteristics that contribute to the character of the area (see Plate 50 and Map 5).



Plate 48 View south along Princess Road



Plate 49 View out of the area along railway tracks



Plate 50 No. 13 Princess Road

II FUTURE MANAGEMENT SUGGESTIONS

II.01 The following issues should form the basis for a management plan for the Strensall Conservation Area. However, all aspects of the designated area identified in the appraisal should be subject to measures for preservation or enhancement, particularly when considering applications for planning permissions within or adjacent to the Conservation Area.

II.02 Consideration should be given to introducing Article 4 (2) directions to the Conservation Area. This would provide additional protection to unlisted buildings within the area and could be used, for example, to resist the replacement of timber doors and windows in unsuitable materials/styles, or the loss of typical features such as gates, fences, walls or other means of enclosure.

II.03 Along The Village there are a number of commercial uses which generate signage and the need for shop frontages. Some of the existing signage and shop fronts are considered inappropriate in the village context. An improvement in the overall design approach, that responds to the village context through the use of sympathetic natural materials, would be

beneficial to the appearance of the designated area.

II.04 A consistent approach to hard surfaces would bring about a significant improvement in the visual appearance of the area. The footpaths and highways require an appropriate choice of materials dependent on their location and functional requirements.

II.05 Within the area there are a variety of lighting column designs. It would be beneficial if the columns were of a consistent design approach and of a human scale. Also, the light emissions should be sympathetic to their rural location.

II.06 Regular tree maintenance is important and there should be encouragement for reinforcing hedges and other mature planting. In addition, it is recommend the Council re-survey the designated area and its immediate setting to establish if any further Tree Preservation Orders would be appropriate.

II.07 It is recommended that the Council suggest to English Heritage that the listing of the New Strensall Bridge be amended to include the associated walls along the river.

APPENDIX A

LISTED BUILDING DESCRIPTIONS

CHURCH LANE (north side)

Church of St. Mary

Grade II

Church. 1865-6 and late C20 addition. J B and W Atkinson. Sandstone and Westmorland slate. West tower, 4-bay nave with south porch, 2-bay chancel with north vestry and C20 parish room. In Geometric style. 3-stage tower with angle buttresses. West front: 2-light window with trefoil window and relieving arch above. Twin belfry windows to each face. Spire. South porch: pointed doorway of single order on shafted responds. 2-light windows to nave, single-light windows to chancel. 3-light window to east end. No internal features of merit. C20 extension not of special interest.

Pevsner N, Yorkshire: The North Riding, 1966

CHURCH LANE (south side)

No. 3

Grade II

House. Mid C18. Brick with pantile roof. 2-cell end-lobby-entry plan. 2 storeys, 2 bays. 4-panel door to right with 4-pane sashes beneath cambered brick arches to left. First floor: 4-pane sashes with timber lintels. Dentilled eaves course. Raised gable ends. End stacks. Tumbling-in to gable ends. Interior: original brick-built inglenook fireplace.

HAXBY MOOR ROAD

Strensall New Bridge (formally listed as Strensall Low Bridge)

Grade II

Bridge. Late C18. Brick with ashlar dressings. Single round-arched humpback bridge. Splayed approaches with piers and stepped stone coping. The only surviving bridge built by the Foss Navigation Company.

SHERRIFF HUTTON

Strensall Bridge

Grade II

Bridge. 1798. Built to designs by John Carr of York. Ashlar. Single segmental arch flanked by buttresses. Solid cambered coped parapet. Ramped approaches with containing walls in matching style. The bridge cost £1,363 3s 2d to build.

THE VILLAGE

War Memorial

Grade II

War memorial, 1922 with later addition. A stone Celtic cross on a rectangular shaft mounted on a tall plinth resting on a 4 step base.

The inscription on the front of the plinth reads: TO PERPETUATE THE MEMORY/OF THE MEN OF THIS PARISH WHO/WERE KILLED, DIED FROM/ WOUNDS OR SICKNESS CONTRACTED/IN THE GREAT WAR/1914-1919, followed by 17 names with regiments, and below: ERECTED BY PUBLIC SUBSCRIPTION/ORGANISED BY THE STRENSALL BRANCH/OF THE BRITISH LEGION.

On the left return on the plinth is inscribed WORLD WAR/1939-1945/REMEMBER, followed by 6 names.

The cross bears a further inscription half way up the shaft reading THEY GAVE/THEIR LIVES/THAT WE/MIGHT LIVE. Above this is deeply incised floral decoration covering the shaft and the head of the cross, with a central rose. Below the inscription is a relief carving of a soldier facing left and looking downwards, with his hands resting on his gun before him. Near his feet are representatives of a field gun and part of a tank. A laurel wreath surrounds his head.

Summary of Importance

War memorials have a very strong historical and cultural significance on both a local and national scale. The memorial cross at Strensall is a fine example with a high standard of craftsmanship in the carving of its decoration and relief figure of a First World War soldier. As a record of the fallen of both World Wars it is clearly of national significance.

THE VILLAGE

(north side, off)

Manor House Farmhouse formally listed as Strensall Hall

Grade II

House. Late C17 in origin, substantially altered C18 and C20. Whitewashed brick, C20 plain tile roof. L-shaped on plan with central-hallway entry. 2 storeys, 5 bays. Early C19 half-glazed door beneath Gothick-traceried fanlight in pedimented doorcase supported on slender columns. Substantial first-floor string course. PVC windows in original openings, beneath elliptical relieving arches throughout. Steeply-pitched hipped roof. Stacks removed at 1984 re-roofing. Interior: late C17 closed string staircase with plain squat balusters. Landing balustrade is a C20 copy.

THE VILLAGE
(north side)

No. 59
(formally listed as The Grange)

Grade II

House. Late C18 remodelled early C19. Brick in Flemish bond, pantile roof. Central-hallway entry, 2 rooms in depth. 2 storeys, 3 bays. 6-panel doorway beneath divided overlight in Doric doorcase with engages columns. Replacement 16-pane sashes beneath cambered brick arches. Heavy timber cornice with paired dentils. Gable coping and end stacks.

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STRENSALL RAILWAY BUILDINGS CONSERVATION AREA APPRAISAL



In association with Strensall with Towthorpe
Parish Council

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APPENDIX A - Listed Building Descriptions

This document was prepared in November 2010 by Woodhall Planning and Conservation on behalf of the City of York Council. The format and content were agreed with the Design, Conservation and Sustainable Development Section of City of York Council, and the document was approved by the Council on

2 LOCATION AND CONTEXT

- 2.01 The Strensall Railway Buildings Conservation Area lies approximately 5 kilometres north of York city centre. Strensall village sits to the south and east of the River Foss and is positioned between the B1363 and A64 (see Map 1). The Strensall Railway Buildings Conservation Area sits within the Strensall village envelope and is located at the eastern end of Strensall, largely to the north of the railway line (see Plate 1).
- 2.02 The designated area is focused around the former station house, its associated signal box and a number of associated residential properties along The Village.
- 2.03 It is estimated that approximately 100 people live within the Conservation Area.



Plate 1 Aerial view

3 TOPOGRAPHY AND LANDSCAPE SETTING

3.01 Strensall village occupies part of a flat open area to the south of the River Foss. The village itself is surrounded by fields to the north, east and west. These areas of open ground enable long distance views towards the village from the north. To the south-east of the village is Strensall Common and Strensall Army Camp.

3.02 The immediate setting of the Strensall Railway Buildings Conservation Area is dominated by twentieth century housing development, the railway line and a small section of agricultural land between the railway line and the houses on the south side of The Village. Glimpses of the surrounding countryside are possible around the former station yard and along the railway tracks. As a result the designated area is enclosed by surrounding development with only the open ground to the south and the small playing field to the west (see Plate 2) providing the opportunity for open views and the sense of separation from Strensall itself.



Plate 2 View of playing to the west

4 HISTORICAL DEVELOPMENT

4.01 The Conservation Area clearly relates to the development of Strensall that occurred as a result of the building of the railway line.

4.02 The York to Scarborough line was opened in 1845 and it is understood that the station house was built in 1848. The 1854-56 Ordnance Survey map illustrates the basic layout of the designated area with The Village, known at this date as Hags Lane, laid out in its current alignment and Lord's Moor Lane extending south from the level crossing.

4.03 Strensall Railway Station is identified on the 1854-56 Ordnance Survey map but it is not clear whether the signal box that survives today is shown at this date. It is also assumed that the platform opposite the signal box was in existence at this date. The coal drops on the north side of the railway line, beyond the platform, are not clearly shown but appear to be part of the coal depot that is identified. Also at this date there appears to be a small group of buildings to the west of the station house on the south side of The Village.

4.04 By 1892 the designated area has not changed significantly. What has

previously appeared to be a small group of buildings on the south side of The Village (now shown as Station Road) to the west of the station house, now appears as only one building, which still survives (see Plate 3). On the north side of The Village two houses appear to have been built at its eastern end opposite the level crossing.

4.05 By the early twentieth century most of the buildings that are within the Conservation Area had been constructed, as shown on the 1912-13 Ordnance Survey map. This shows houses on both sides of The Village (see Plate 4), although a gap exists on the north side. Also, by this date the signal box is clearly shown (see Plate 5) and the brick and tile works to the north had been established.

4.06 Little changed within the designated area during the remainder of the twentieth century. The cinder track leading to the brick and tile works was adopted as highway and Lords Moor Lane, leading south from the signal box, became a formal extension to the main road. In 1930s Strensall Station closed to regular timetabled passengers, but Sunday



Plate 3 Early building



Plate 4 View of houses on the north side of The Village



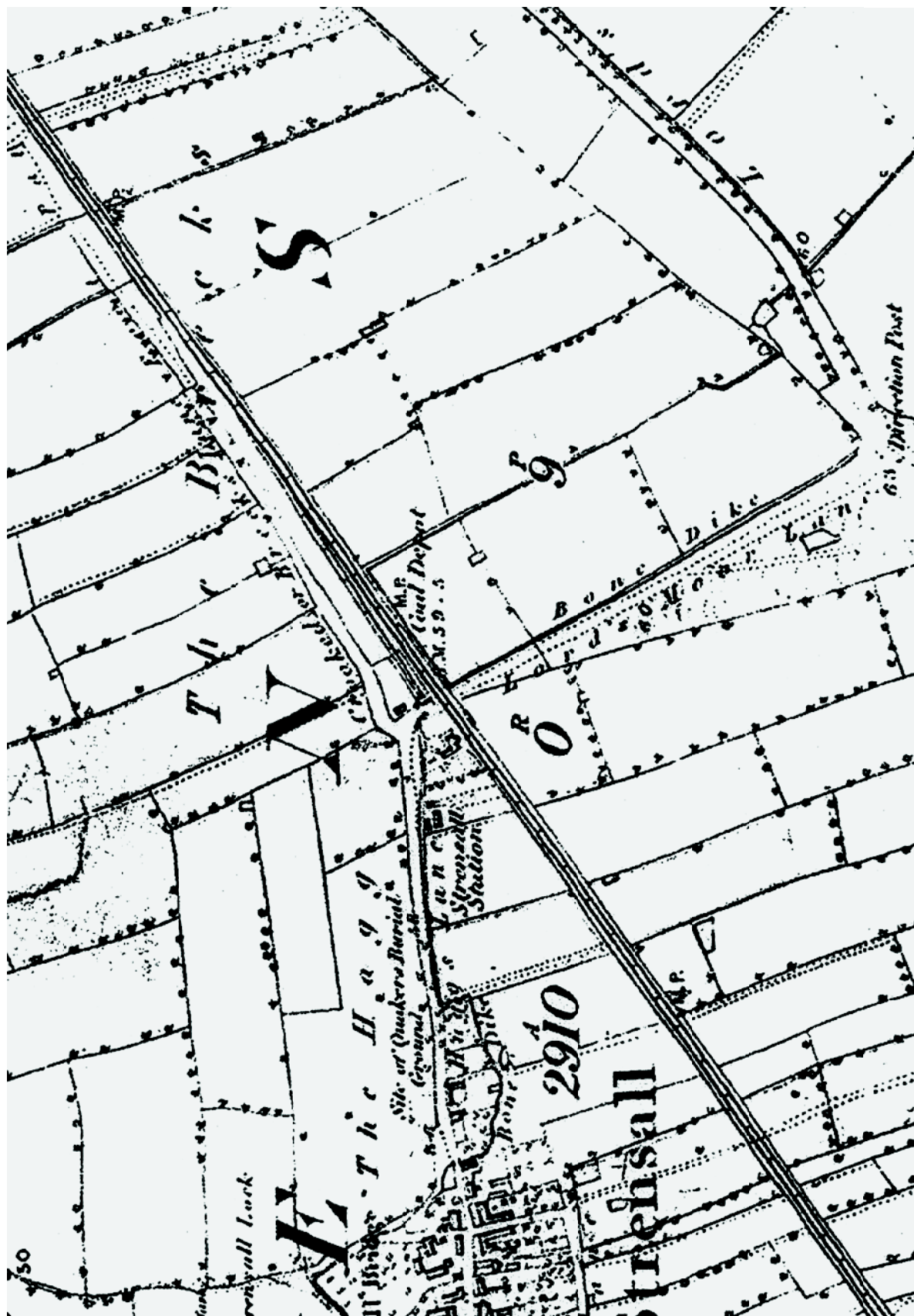
Plate 5 The signal box

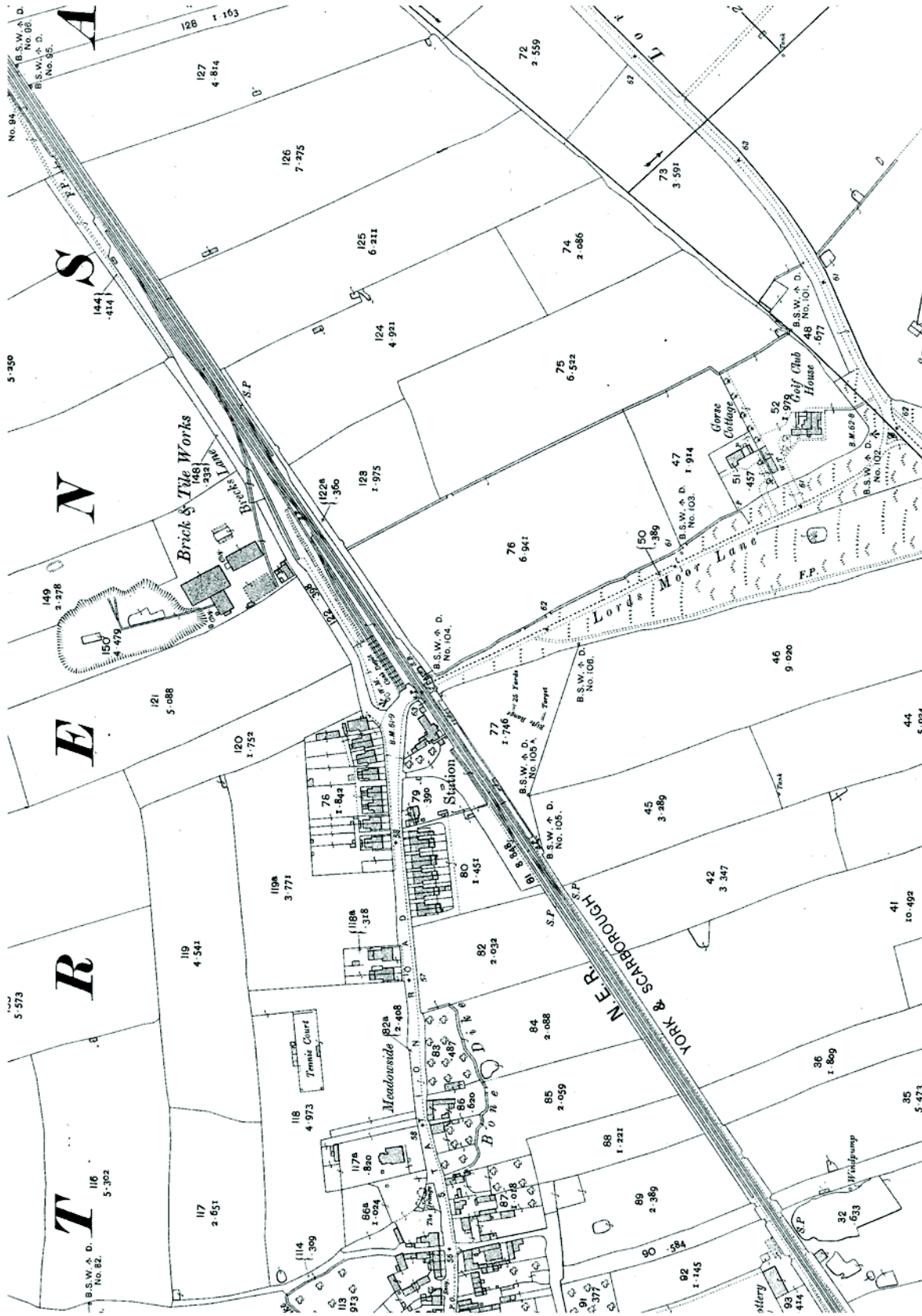
services and excursions operated until the 1950s. The station was also used by the nearby army base during World War Two. The station house was brought back into use as a private residential unit at some stage during the second half of the twentieth century.

4.07 No. 101 The Village was built in 1937 to serve as the local doctor's house and surgery (see Plate 9) and at about the same time the Northfields development was constructed to the north. Later developments within the Conservation Area generally involved the construction of individual houses (see Plate 6). Finally, "Station Road" was re-named The Village during the second half of the twentieth century.



Plate 6 Oblique view of 128a on right hand side





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5 CONSERVATION AREA BOUNDARY

5.01 One of the requirements of an appraisal is a review of the Conservation Area boundaries to establish whether changes would be appropriate. Therefore the extent of the existing designated area has been reviewed and areas around the existing Conservation Area have been studied to ascertain whether any new areas should be included or existing areas excluded.

Existing boundary

5.02 The current boundary of the designated area is illustrated on Map 2. The eastern end of the designated area is focused around the former station house together with its associated signal box and level crossing. It is also includes what are understood to be the former sidings to the south–west. The reminder of the designated area is focused around housing on both sides of The Village which runs in an east/west direction.

Possible alterations

5.03 There are no areas which are considered to be included inappropriately within the current boundary.

Possible additions

5.04 Two areas are being put forward for inclusion in the Conservation Area: the

former station yard and contemporary railway housing to the west. To the north of the railway line adjacent to the level crossing is the former station yard. This includes the coal drops that were associated with the railway (see Plate 7) These are in form of a series of uncovered brick bays that abut the wall that was part of the old platform, which has been demolished. The 1854-56 Ordnance Survey map indicates that there was a coal depot at the station. In view of their historic association with the railway it is considered that the former station yard is worthy of inclusion within the designated area (Area A on Map 2). The former station yard is directly in line of view from the west when traveling along the main road and therefore any development must be strictly controlled.

5.05 Beyond the western end of the current boundary on the north side of The Village are five residential properties of interest: Nos. 93 - 101 (Area B on Map 2). Nos. 93-99 (see Plate 8) are a two villa pairs of similar age and form to the other villa pairs in the designated area, although Nos. 97 and 99 have lost most of their original details. No. 101 is an interesting and well designed detached house, dated 1937 (see Plate 9). It was purpose built as a doctor's



Plate 7 View of former station yard



Plate 8 Nos. 93-99 The Village



Plate 9 No. 101 The Village

house and surgery. In view of the quality of these buildings it is recommended they are also included in the designated area.

- 5.06 It is also proposed to include No. 103 The Village in the designated area. This is a single storey late twentieth century house (see Plate 10). Although it is of no special interest it is considered appropriate to include the house in the designated area in order to sensibly connect the proposed extension to the existing Conservation Area boundary.

Recommendations

- 5.07 In view of all the above, it is considered that the boundary of the designated area be altered to include the former station yard associated with the railway (Area A on Map 2) and numbers 93 - 103 The Village (Area B on Map 2).



Plate 10 No. 103 The Village



Map 2 - Current Conservation Area boundary

6 GENERAL CHARACTER

6.01 This section considers the general character of the whole of the Conservation Area.

Use

6.02 The predominant use of the buildings within the area is residential, with the exception of the railway signal box located at the level crossing at the east end of the designated area (see Map 3 and Plate 11). The former station yard (Area A of the proposed additions) was used as a builders yard with informal office until recently. Within Area B of the proposed addition there are two small areas of landscaped open space on either side of the junction of Northfields with The Village.

Qualities of the Buildings

6.03 Within the Conservation Area there is only one listed building (see Map 4 and in Appendix A). This is the former station house, which is located directly on the north side of the railway line and was built in circa 1848 for the York and North Midland Railway Line. The building is two storeys in height and is constructed of local brick with stone dressings and arch-headed windows under a shallow pitched overhanging slate roof. It has a main range with a central bay that breaks forward towards the railway line and is typical of the early generation of railway buildings (see Plate 12).

6.04 The only other building that relates directly to the railway is the signal box on the south side of the level crossing (see Plates 5 and 11). This has a brick base with a distinctive glazed first floor.

6.05 On the south side of The Village Nos. 126 & 128 are the only examples of an early buildings within the designated area. The house is semi-detached with a simple form, is constructed of bricks with no decorative detailing and has a pantiled roof (see Plate 3).

6.06 The majority of the houses within the designated area are late Victorian or early Edwardian in style. These are two storeys in height, constructed of red brick and have pitched slate roofs (see Plate 13). The predominant building form is small villa pairs but there is one example of a short terrace containing five units on the north side of The Village. These houses sit behind small front gardens that are defined by a low brick wall topped with a stone coping. The houses are positioned close together which creates a fine linear urban grain and also results in a high density of development.

6.07 Many of these later houses have decorative detailing, in contrast to the



Plate 11 View towards the signal box



Plate 12 The former station house



Plate 13 Example of a villa pair

older buildings in the area. Dented brick coursing above openings or a string course of white contrasting brick work provide interest to the façade (see Plate 13). Canted bay windows are a feature of these houses and these provide a rhythm to the façade and the streetscene. The front doors are highlighted through the use of timber porches (see Plate 14). These porches often have elaborate carved panels and brackets. Many of the houses retain their original vertical sliding sash windows and timber doors.

6.08 There is one later distinctive house (No. 101 the Village), dated 1937, which also contributes to the appearance of the area (see Plate 9). This is within Area B of the proposed additions to the designated area at the west end of The Village. The house is constructed with red brick under hipped pantile roof. It has a simple form with little elaboration, with the exception of the bracket porch over the entrance door.

Spaces and views

6.09 Within the designated area there are a limited number of spaces due to the form of the area and its small size. The main spaces are the roads and their associated elements. The exceptional width of The Village at its east end indicates that this was a terminus for the bus and for delivery vehicles as there were no through roads until the adoption of Lords Moor Lane and

Brecks lane in the mid twentieth century. The bus terminus has been re-located north. None of the pavements retain their original materials, which have been replaced with modern concrete kerbs and tarmac surfacing. However, the rubber road surface across the level crossing does provide interest and variety to the area and reflects its function.

6.10 There are two small open grassed spaces, each planted with ornamental trees within Area B of the proposed additions to the designated area on either side of the junction between Northfields and The Village. These spaces provide a break in the built form of the area and the trees create an attractive area of landscape (see Map 5).

6.11 Views within the designated area are predominantly along The Village to either end of the Conservation Area (see Map 5 and Plate 15). Similarly, views are possible into and out of the designated area along The Village and other roads. Also, there are significant views out of the designated area from the level crossing looking along the railway tracks in both directions (see Plate 17). The level crossing also provides an oblique view to the former station house (see Map 5 and Plate 12).

6.12 A significant view into the designated area exists looking north along Lord's Moor Lane. This view is framed by the signal box and



Plate 14 Decorative bracketed porches

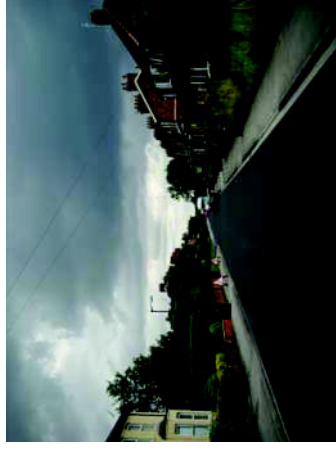


Plate 15 View east along The Village



Plate 16 View into the designated from Lord's Moor Lane

gates at the level crossing and the vista is closed by the houses on the north side of The Village (see Plate 16). The level crossing acts as a gateway as it marks the entry into the area from the south and the signal box acts as a local landmark (see Map 5).

- 6.13 The former station yard (Area A of the of the proposed additions to the designated area) is a private open space of historic significance. It contains coal drops and other storage facilities associated with the railway that comprise of a series of large uncovered brick-built bays (see Plate 7).

Soft Landscaping

- 6.14 The streetscene along the Village is softened by the trees, shrubs and hedges located in the front gardens of the houses. The large mature trees in the garden area of the former station house at the east end make a very significant contribution to the character and appearance of the Conservation Area as do other tree groups and ornamental trees at road junctions (see Plate 18 and Map 5).

Neutral and Negative factors

- 6.15 Within the designated area there are a limited number of neutral and negative factors (see Map 4).
- 6.16 Within Area B of the proposed addition to the Conservation Area, Nos. 97 and 99 The

Village are a villa pair, which has its brickwork covered with render; this has also resulted in the loss of much of its original detailing. Although the building still displays its original form which could be re-instated. As a result, it is considered that the building is a neutral factor within the designated area.

- 6.17 In addition, some houses have unfortunately lost their original timber window frames and front doors. Other houses have lost their front boundary walls and the front gardens have been replaced with hard surfacing to provide vehicle parking.

- 6.18 On the south side of The Village, to the east of the former station house is a large modern house set back from the general building line with an L-shaped footprint and large dormer window. Unfortunately the effects of its exposed location and its uncharacteristic building typology and dormer window represent a minor intrusion into the area (see Plate 19). Similarly the bungalow at No. 103 The Village (which is within Area B of the proposed addition to the designated area) is also uncharacteristic of the remainder of the Conservation Area (see Plate 10).



Plate 17 Railway tracks



Plate 18 Trees in garden area of the former station house



Plate 19 View of modern house on the south side of The Village





Map 4 - Building Assessment



Map 5 - Spatial analysis

7 FUTURE MANAGEMENT SUGGESTIONS

- 7.01 The following issues should form the basis for a management plan for the Strensall Railway Buildings Conservation Area. However, all aspects of the area identified in the appraisal should be subject to measures for preservation or enhancement, particularly when considering applications for planning permissions within or adjacent to the Conservation Area.
- 7.02 Consideration should be given to introducing Article 4 (2) directions to the Conservation Area. This would provide additional protection to unlisted buildings within the area and could be used, for example, to resist the replacement of timber doors and windows in unsuitable materials/styles, or the loss of typical features such as gates, fences, walls or other means of enclosure.
- 7.03 The reinforcement of hedges and other mature planting. In addition, it is recommended that the Council re-survey the designated area and its immediate setting to establish if any further Tree Preservation Orders would be appropriate.
- 7.05 The former station yard to the north of the railway line has an open character which needs to be retained should the site be redeveloped in the future.

- 7.03 Within the area there are a variety of lighting column designs. It would be beneficial if the columns were of a consistent design approach, of a human scale. Also, the light emissions should be sympathetic to a rural location.
- 7.04 Regular tree maintenance is important and there should be encouragement for

APPENDIX A

LISTED BUILDING DESCRIPTIONS

THE VILLAGE
(south side)

No. 130 (Station House)

Grade II

Station. Now house. C1848. Probably by G T Andrews for the York and North Midland Railway. Brick, stone dressings and Welsh slate roof. Main range with central bay breaking forward, addition to right and cross wings to rear. 2 storeys, 5 bays with single-storey 2-bay addition to right. Stone plinth. Central canted bay beneath wide eaves flanked by sashes with glazing bars beneath cambered brick arches. Right extension: 2-panel door with overlight beneath segmental arch, with sash with glazing bars beneath segmental arch to left. First floor: continuous stone band at sill level. Pair of round-arched plate glass sashes flanked by sashes with glazing bars beneath cambered brick arches, with window opening to extreme left blocked. End and ridge stacks. The York to Scarborough line was opened in 1845.

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TOWTHORPE CONSERVATION AREA APPRAISAL



In association with Strensall with Towthorpe
Parish Council

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The name of designated area is proposed to amende from Towthorpe Village to the Towthorpe Conservation Area.

APPENDIX A - Listed Building Descriptions

This document was prepared in November 2010 by Woodhall Planning and Conservation on behalf of the City of York Council. The format and content were agreed with the Design, Conservation and Sustainable Development Section of City of York Council, and the document was approved by the Council on

I INTRODUCTION

1.01 A conservation area is defined by the Planning (Listed Buildings and Conservation Areas) Act 1990 as an area of special architectural or historic interest, (the character or appearance of which) is desirable to preserve or enhance. Section 69 of the Act requires Local Planning Authorities to identify and designate conservation areas within their geographical boundaries and to formulate, in conjunction with the public, proposals for their preservation and enhancement (section 71 and 72 of the Act). The Act places a duty upon Local Planning Authorities to review the extent of the conservation areas.

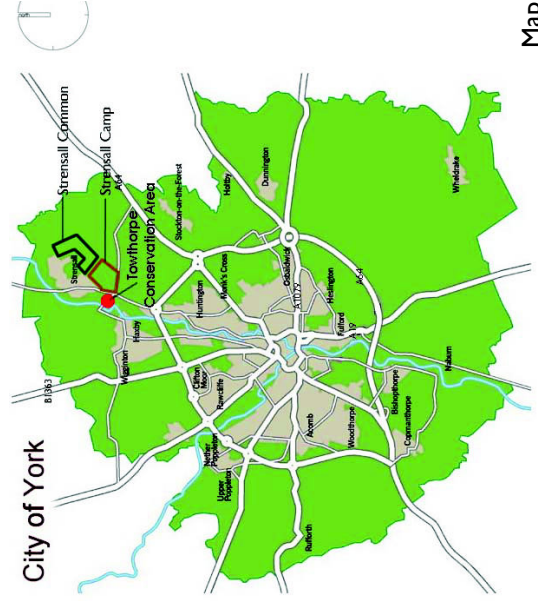
1.02 Although the Conservation Area is named as “Towthorpe Village”, Towthorpe is actually a small agricultural hamlet, located to the north of York (see Map 1). The Towthorpe Village Conservation Area was designated in 2001, with the boundaries as shown on Map 2 within Appendix A. The whole area is less than 9 hectares.

1.03 This document sets out the findings of a character appraisal of the Towthorpe Village Conservation Area and can be regarded as being in four parts. The first

part (Chapters 1-4) sets the scene by analysing the history and baseline factors of the conservation area. The second part (Chapter 5) reviews the extent of the Conservation Area as originally designated and explains the reasons for the proposed revision of its boundaries. The third part, (Chapter 6) describes the general character of the area as revised. The final part (Chapter 7) contains suggestions for future management.

I.04 The assessment is based on field work in August and September 2010 and is limited to the buildings and areas visible from the public domain.

1.05 Towthorpe Village Conservation Area Appraisal was approved as a supporting document to the conservation policies included in the City of York's draft Local Plan (incorporating the fourth set of changes) by the Planning Committee of City of York Council on *to be inserted*. The Conservation Area Appraisal will also be used to inform the emerging Local Development Framework.



Map I

2 LOCATION AND CONTEXT

2.01 The Towthorpe Village Conservation Area lies approximately 5 kilometres north of York city centre. Towthorpe sits to the east of the River Foss and is positioned between Haxby, approximately 1 kilometre to the west, and Strensall which is approximately 1.5 kilometres to the north east (see Map 1).

2.02 The designated area sits on both sides of Towthorpe Road and is focused around a small agricultural hamlet of long standing (see Plate 1). The designated area around and surrounding land forms part of the City of York Green Belt, and is recognized in the emerging Local Development Framework as an area preventing coalescence (Policy G1).

2.03 It is estimated that approximately 15 people live within the Conservation Area.



Plate 1 Aerial view

3 TOPOGRAPHY AND LANDSCAPE SETTING

3.01 Towthorpe occupies part of an open area to the east of the River Foss. The buildings appear to be set on a slight rise in the landscape when viewed from the west. This topography has contributed to the north/south alignment of the settlement and caused the sharp bends of the approach roads. The hamlet itself is surrounded by fields. These areas of open ground enable long distance views towards the village from the north, south and west.

3.02 The immediate setting of the designated area is dominated by open agricultural land (see Plate 2). To the north side of Towthorpe Road there is an unusual mounded field which appears to be man made. To the east, along the south side of Towthorpe Road, there are a number of modern residential properties that are outside the Conservation Area. To the east of Towthorpe village is Strensall Army Camp. To the north-east corner is a heavily wooded area beyond Low Farm.



Plate 2 View towards the Conservation Area looking north along Towthorpe Road

4 HISTORICAL DEVELOPMENT

4.01 Information about the early history of Towthorpe is limited. It was a well-established settlement by the eleventh century but may have much earlier origins as there is evidence for pre-historic and Roman activity in the wider area.

4.02 The township of Towthorpe was owned by the Count of Mortain in 1086 and at the beginning of the fourteenth century was held by the de Mauleys. The moat on the western edge of the Conservation Area is probably the site of the early manor house, which may have made use of the River Foss for protection. There is evidence of ridge and furrow (indications of medieval ploughing) to the north-west of the hamlet. There appears to have been a change in land tenure by the early fifteenth century, which may indicate a significant reduction in the size of the settlement.

4.03 The size of the hamlet may have remained relatively unaltered since this late medieval period, although there was a substantial period of rebuilding in the eighteenth century, probably linked to agricultural improvements. Most of the farm houses appear to date from this period, although the associated farm buildings are generally later.

4.04 The location of Grange Farm, away from the road and the other farms may suggest that it had a different function or status (see Plate 3). The 1854 Ordnance Survey Map suggests that it was a focal point at the south end of the hamlet.

4.05 There have been few developments within Towthorpe during the twentieth century. The farm complex north of Manor Farm (see Plate 4) appears to have been absorbed by one of the other farms towards the beginning of the century as by 1911 its house had been sub-divided into three cottages. During the latter half of the twentieth century the cottages have been altered to storage accommodation. One bungalow (Beith Acre - now massively extended) was erected in the 1920s to the south of Low Farm and other similar properties were created to the east of Grange Farm (outside the designated area).

4.06 The farmsteads catered for a mixed agricultural economy. In the last few decades agricultural activity has reduced within the hamlet and many of the buildings have become derelict. However, within the past two years, the farmhouses at Manor Farm and Low Farm have been restored. (see Plate 5).



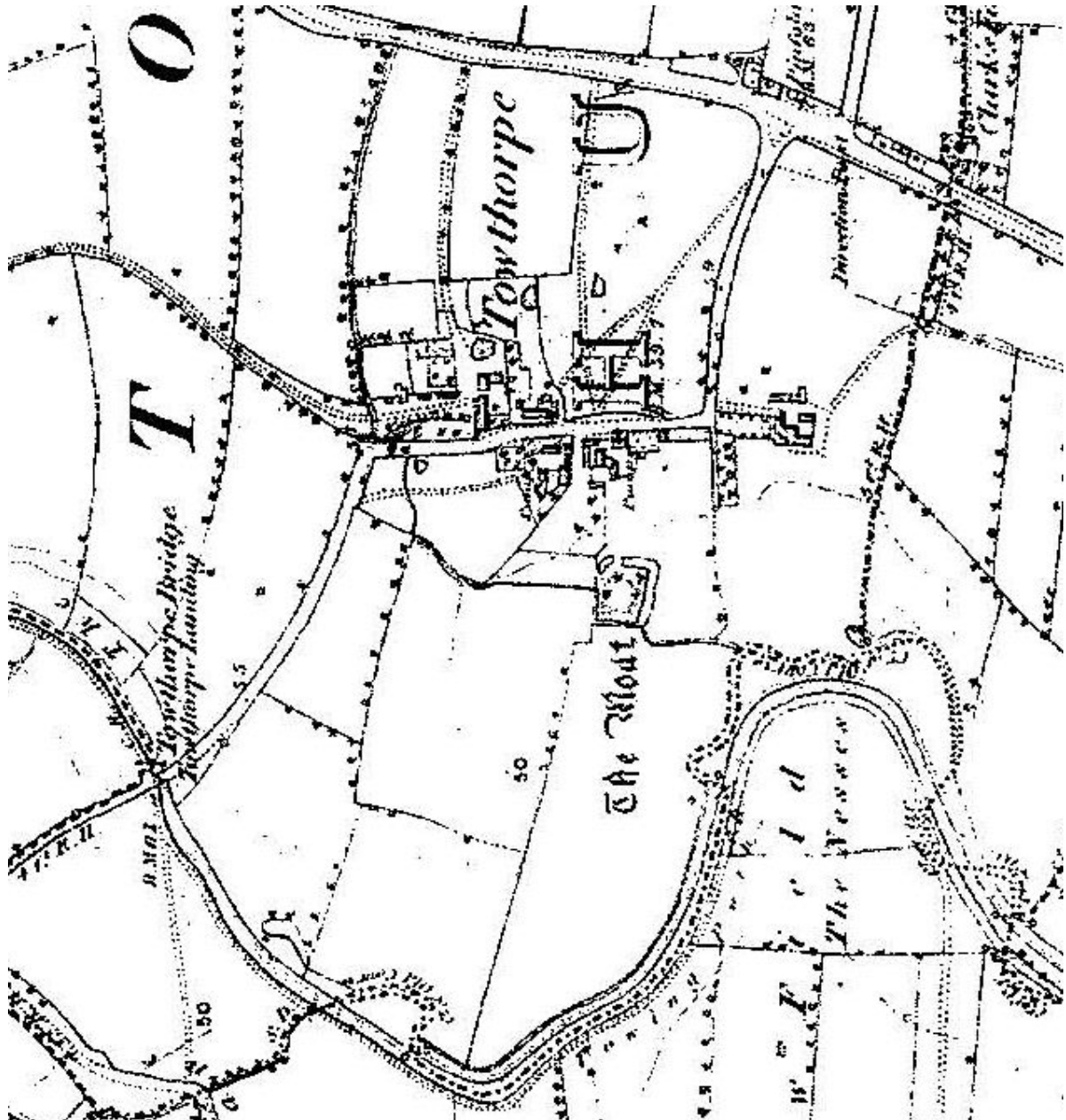
Plate 3 View towards Grange Farm



Plate 4 Farm buildings at Manor Farm

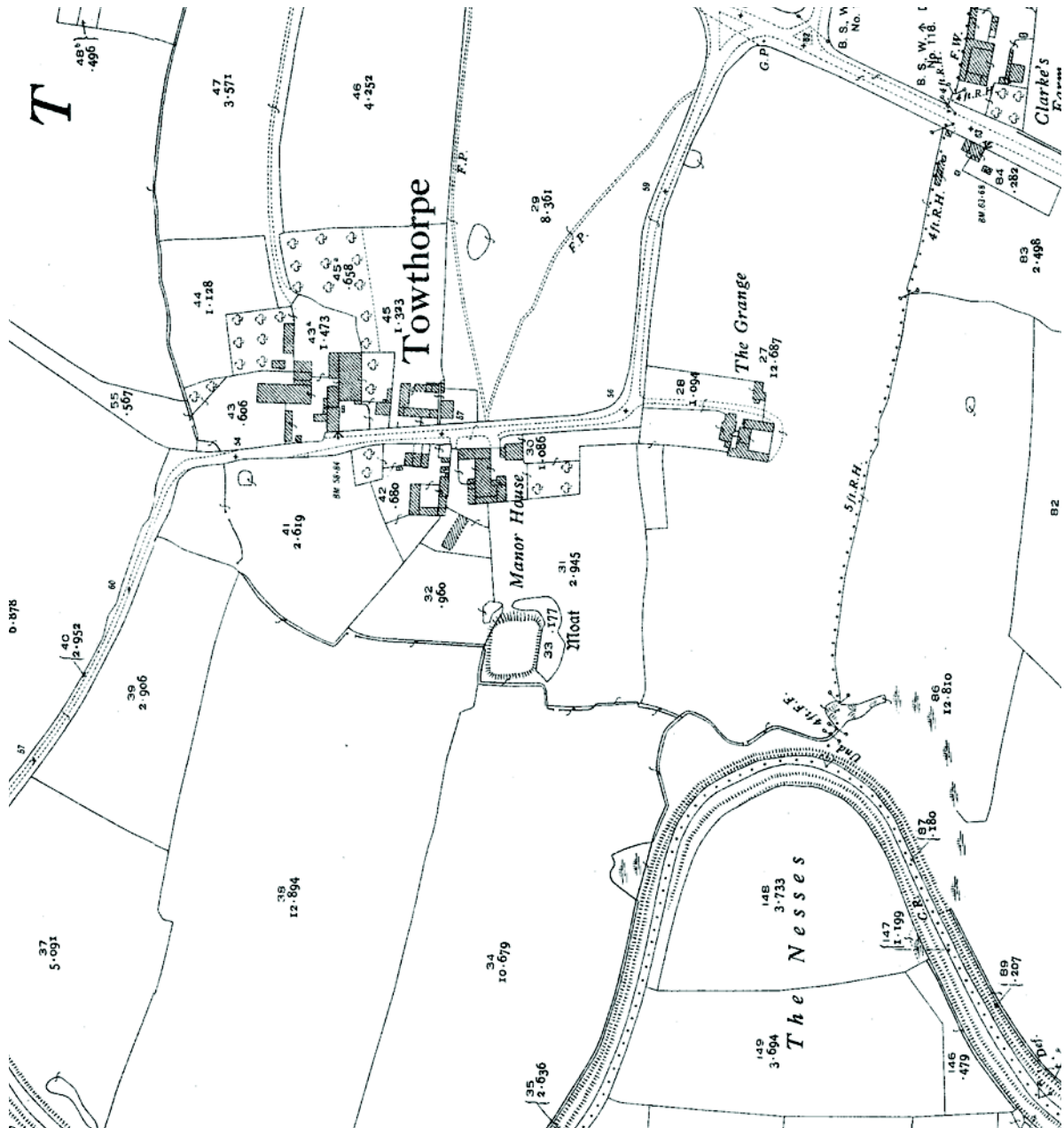


Plate 5 Manor farmhouse



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1852 Ordnance Survey Map



1930 Ordnance Survey Map

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5 CONSERVATION AREA BOUNDARY

- 5.01 One of the requirements of an appraisal is a review of the Conservation Area boundaries to establish whether changes would be appropriate for there to be any changes. Therefore the extent of the existing designated area has been reviewed and areas around the existing Conservation Area have been studied to ascertain whether any new areas should be included or existing areas excluded.

Existing boundary

- 5.02 The current boundary of the designated area is illustrated on Map 2 in Appendix A. The Conservation Area is focused around five farm complexes most of which are no longer in agricultural use (see Plate 6). Grange Farm stands in a relatively isolated location to the south of a bend in Towthorpe Road (see Plate 7) but the other four former farm complexes, together with one other property, form a group on either side of the road further north. The designated area also includes open land on the west side of the road.

Possible alterations and additions

- 5.03 There are no areas which are considered to be included inappropriately and no further additions to the boundary are recommended.

Recommendations

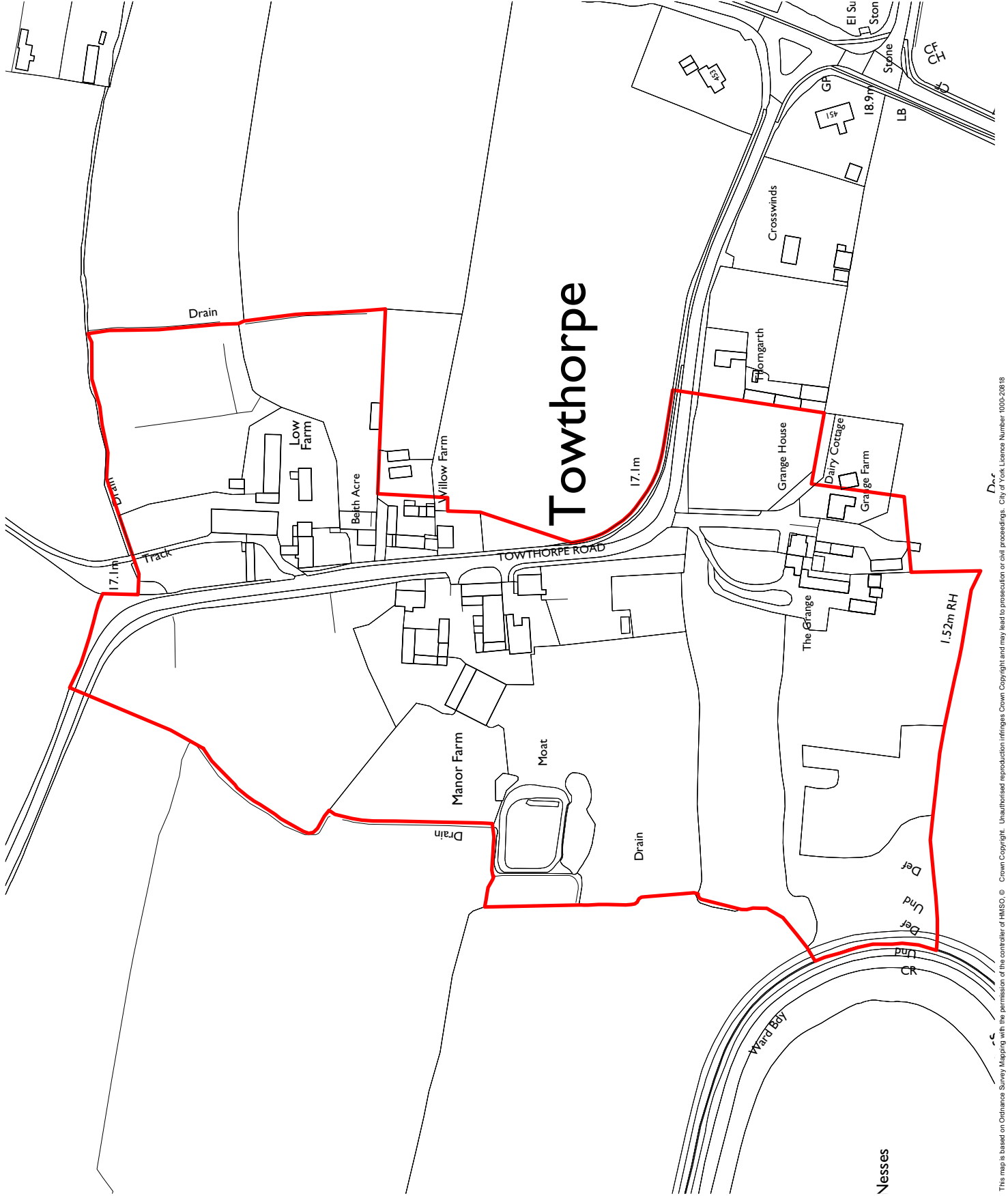
- 5.04 In view of all the above, it is considered that the existing boundaries of the area are appropriate and no alterations are recommended.



Plate 6 View towards Manor Farm



Plate 7 View towards Grange Farm



6 GENERAL CHARACTER

6.01 This section considers the general character of the whole of the Conservation Area.

Use

6.02 The predominant use of the buildings within the area are residential and agricultural (see Map 3). The agricultural and related employment uses have declined and as a result many of the farm buildings are vacant and derelict. The small number of properties and the limited level of activity within the Conservation Area is an essential aspect of its character.

Qualities of the Buildings

6.03 The buildings tend to be large in scale and grouped to form enclosed yards. These clusters are separated by generous open space associated with the agricultural use. A number of the buildings retain evidence of height increase or sub-division demonstrating continuous adaptation of the buildings over a period of time.

6.04 Within the Conservation Area there is only one listed building (see Map 4 and Appendix A). This is Low Farmhouse which is a mid eighteenth century farmhouse constructed of red clamp brick under a pitched pantile roof with its

principal elevation facing south. This is a typical farmhouse of its period, with a simple form, small horizontal sliding sash windows, stepped eaves courses and raised gables (see Plate 8). The other farmhouses display similar form and detailing, although window types have been changed to vertical sliding sashes. The farmhouse north of Manor Farm appears to earlier and may well be of special interest, although it has been altered and is currently derelict.

6.05 The farmhouses have different relationships with the road. At Low Farm and Willow Farm the houses are at right angles to the road. Manor Farm has a house that faces east across the road, while Grange farm is located well away from the road and is accessed by a long drive.

6.06 All of the farms had ranges of agricultural buildings to either house livestock or to store materials, such as hay. Some buildings have been re-used as workshops. The earlier buildings are constructed with brick under a pitched roof whilst the later buildings are portal framed structures covered with metal (see Plates 4, 9



Plate 8 Low Farmhouse



Plate 9 Barn at Manor Farm



Plate 10 Barns at Low Farm

&10). Most of the traditional buildings within the Conservation Area make a positive contribution to its character and appearance. Due to their former agricultural use many of the buildings are surrounded by open yards which results in a low density of built form.

6.07 Beith Acre was originally constructed as a modest, hipped roof bungalow but a very large two-storey extension has been added, which makes the building appear incongruous (see Plate 11).

Spaces and views

6.08 Within the designated area the defined spaces are limited to the immediate curtilages of the farmhouses and other dwellings. These spaces are typically garden areas or yards associated with former agricultural uses (see Plate 4). Along the southern section of Towthorpe Road there are sections of modern tarmac pavements, but elsewhere there are just grass verges.

6.09 Views within the designated area are predominantly along Towthorpe Road to either end of the Conservation Area (see Map 5 and Plate 12). Significant views into the designated area are possible from across the open areas to the north, south and west. Also, as a result of the low density of the built form and the open

layout the area, many views are possible between the buildings.

Soft Landscaping

6.10 The streetscene along Towthorpe Road is softened by many trees and hedges (see Plate 12 and Map 5). As noted above, grass verges exist on both sides of the northern section of Towthorpe Road.

Neutral and Negative factors

6.11 Within the designated area there are a number of neutral factors and negative factors (see Map 4). Beith Acre has is an untypical bungalow which is made more obvious by the contrasting mass of the rear extension. In addition, the loss of a the front boundary enclosure also creates a gap in the streetscene. Also, some of the modern farm buildings are negative factors as a result of their scale and non-traditional materials.

6.12 The recent boundary walls erected at Manor Farm and Low Farm (see Plates 5 and 13) are inappropriate to the area as a result of their design and detailing. The cumulative impact of such small-scale changes can quickly undermine the special qualities of the Conservation Area.



Plate 11 Beith Acre

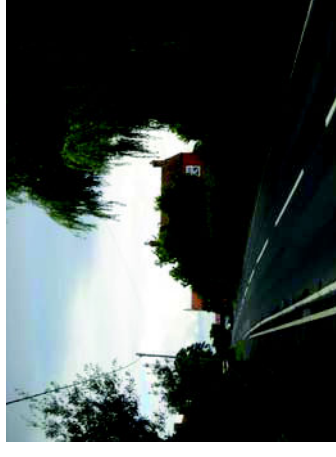
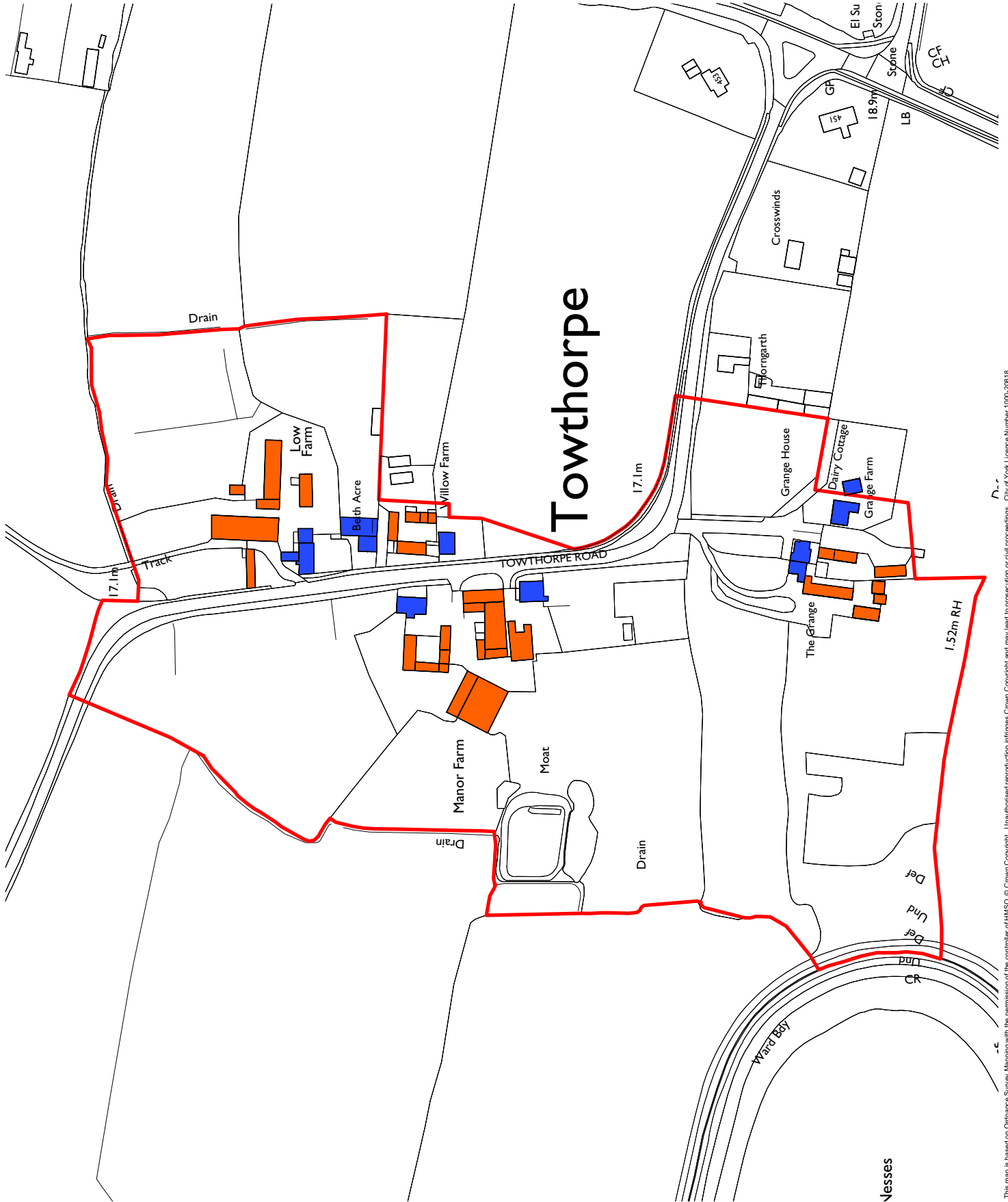
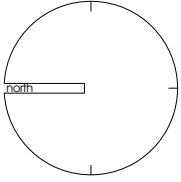


Plate 12 Trees and hedges in the streetscene



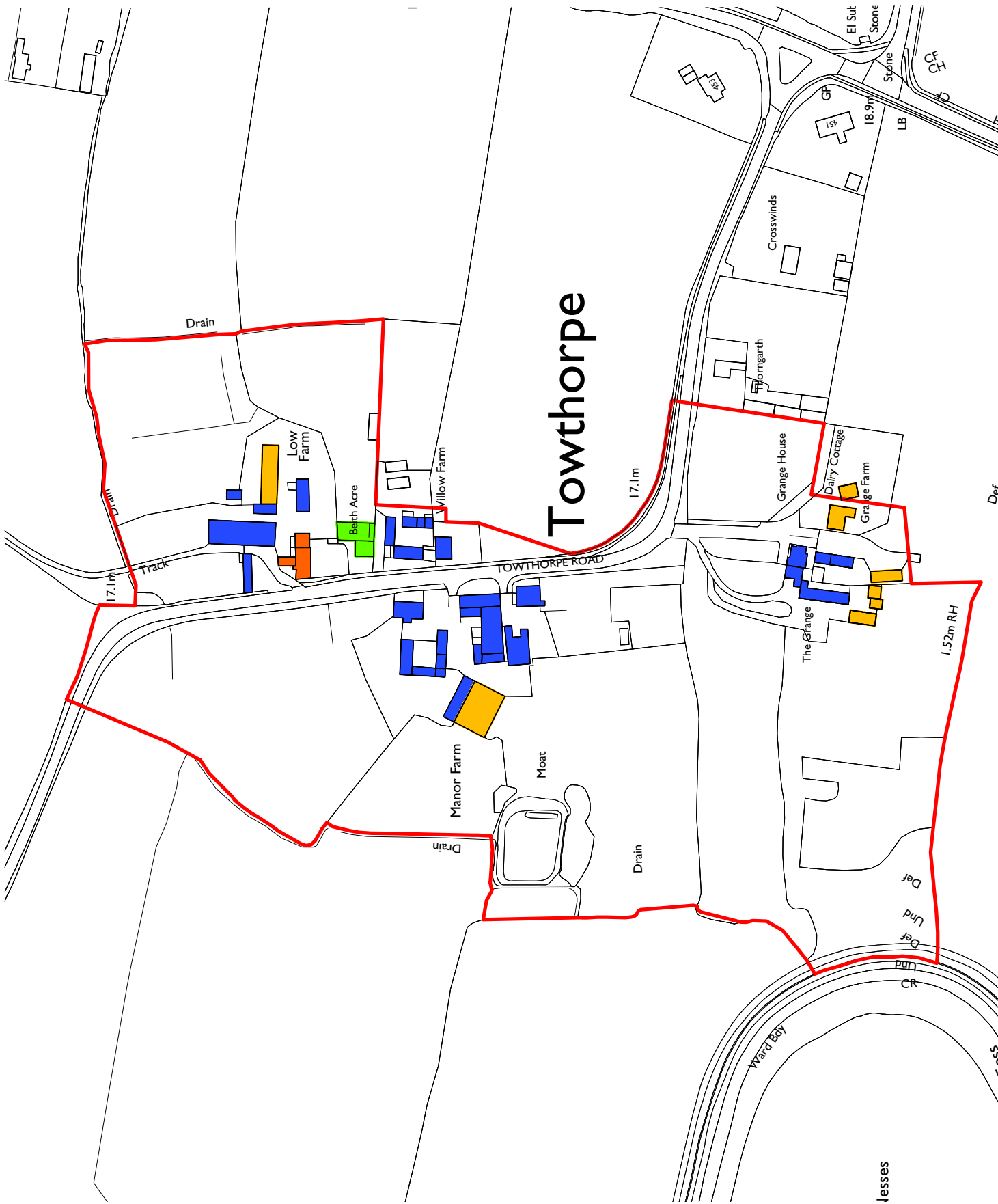
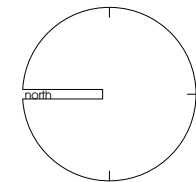
Plate 13 Boundary wall at Low farmhouse



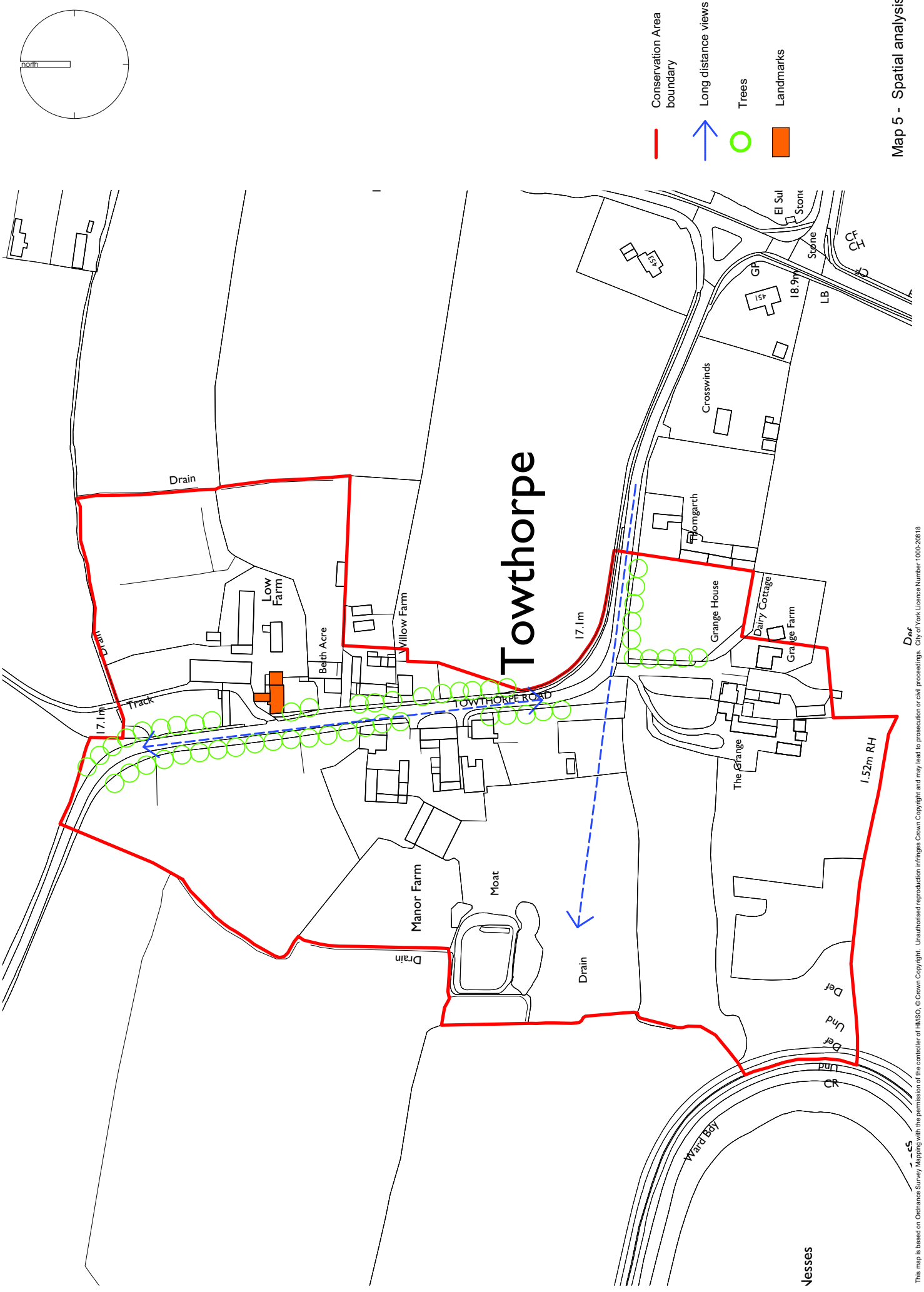
- Conservation Area boundary
- Residential
- Agricultural

Map 3 - Existing uses

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Map 4 - Building Assessment



- Conservation Area boundary
- Long distance views
- Trees
- Landmarks

Map 5 - Spatial analysis

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7 FUTURE MANAGEMENT SUGGESTIONS

- 7.01 The following issues should form the basis for a management plan for the Towthorpe Conservation Area. However, all aspects of the area identified in the appraisal should be subject to measures for preservation or enhancement, particularly when considering applications for planning permissions within or adjacent to the Conservation Area.
- 7.02 The major issue for the Towthorpe Conservation Area is the future of the derelict buildings, particularly the many traditional farm buildings. This would lead to pressure for the future of these buildings to be secured by residential use. However, this is likely to lead to a substantial increase in the number of dwellings within the hamlet, which would destroy its essential character. Alternative employment uses should be encouraged because of the close proximity of the York ring road. Although the relative isolation of the hamlet may mitigate the viability of such proposals and sustainable transport issues would need to be addressed. Otherwise such uses may lead to an unacceptable increase in traffic within the hamlet. One alternative is to encourage the development of a few very large residential units that can make use of both the traditional farmhouses and

their associated farm buildings.
- 7.03

It is recommended that the Council suggest to English Heritage that the farmhouse north of Manor Farm should be considered for inclusion on the List of Buildings of Special Architectural or Historic Interest.
- 7.04

Consideration should be given to introducing Article 4 (2) directions to the Conservation Area. This would provide additional protection to unlisted buildings within the area and could be used, for example, to resist the replacement of timber doors and windows in unsuitable materials/styles. It would also allow control of the design and materials used for new boundary walls.
- 7.05

Regular tree and hedge maintenance is important and there should be encouragement for reinforcing the hedges that make a key contribution to the Conservation Area. In addition, it is recommended that the Council re-survey the designated area and its immediate setting to establish if any further Tree Preservation Orders would be appropriate.

APPENDIX A

LISTED BUILDING DESCRIPTIONS

Towthorpe Road (east side)

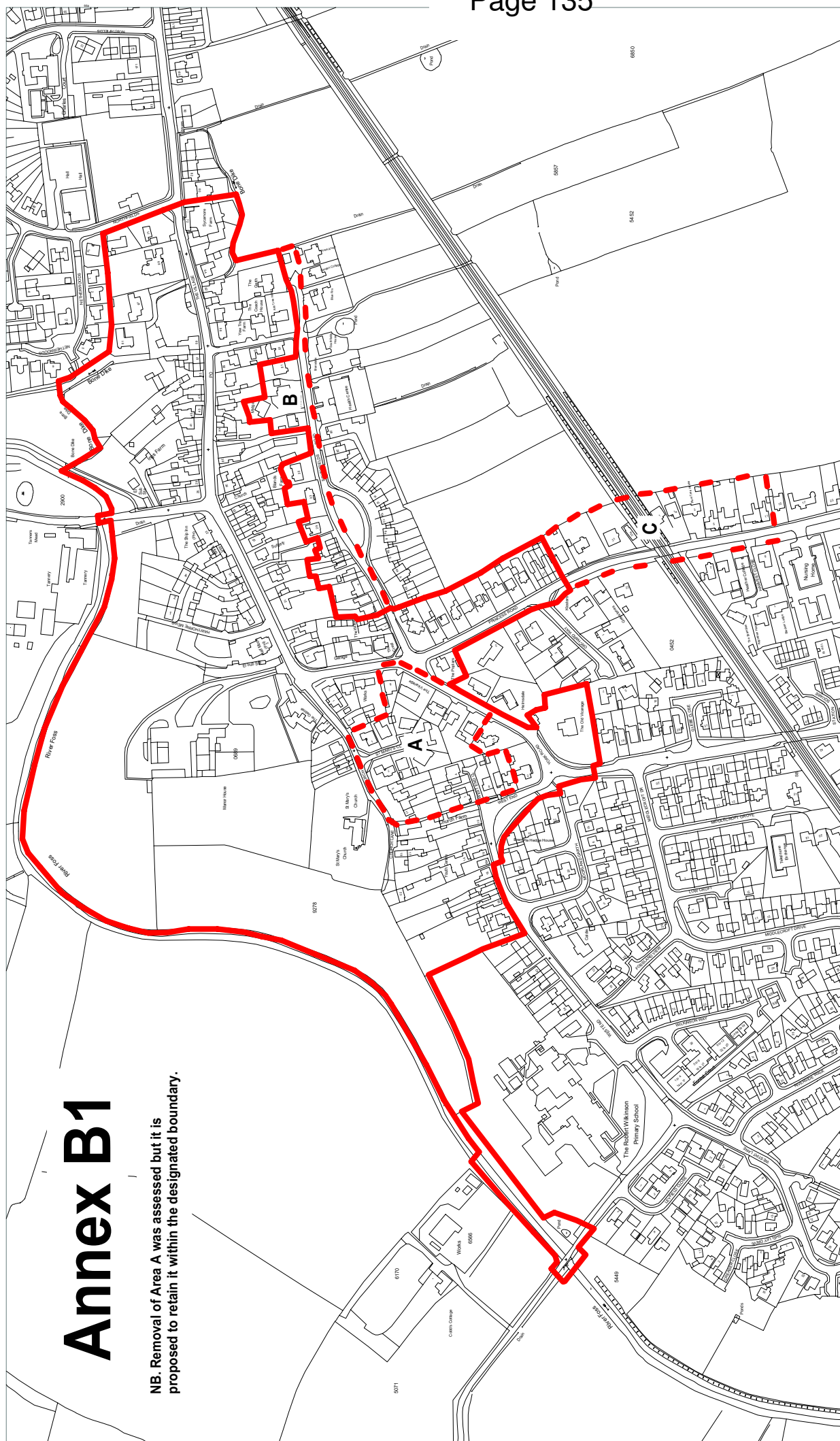
Low Farmhouse

Grade II

House. Mid C18. Brick with pantile roof. Direct entry, 3-cell plan. 2 storeys, 3 first-floor windows. 2-light casement beneath flat brick arches throughout. C20 half-glazed door to left; window in blocked doorway to right. Stepped eaves course. Raised gable ends. End and ridge stacks, that to left rebuilt. Interior contains chamfered spine beams, C18 panels doors throughout, an C18 splat baluster staircase and a C19 range in the kitchen.

Annex B1

NB. Removal of Area A was assessed but it is proposed to retain it within the designated boundary.



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Existing Conservation Area Boundary

Proposed Amendments to
Conservation Area Boundary

STRENSALL - CONSERVATION AREA 23 - Proposed Amendments

DATE 29/10/2010
Drawing No.

CA23PE

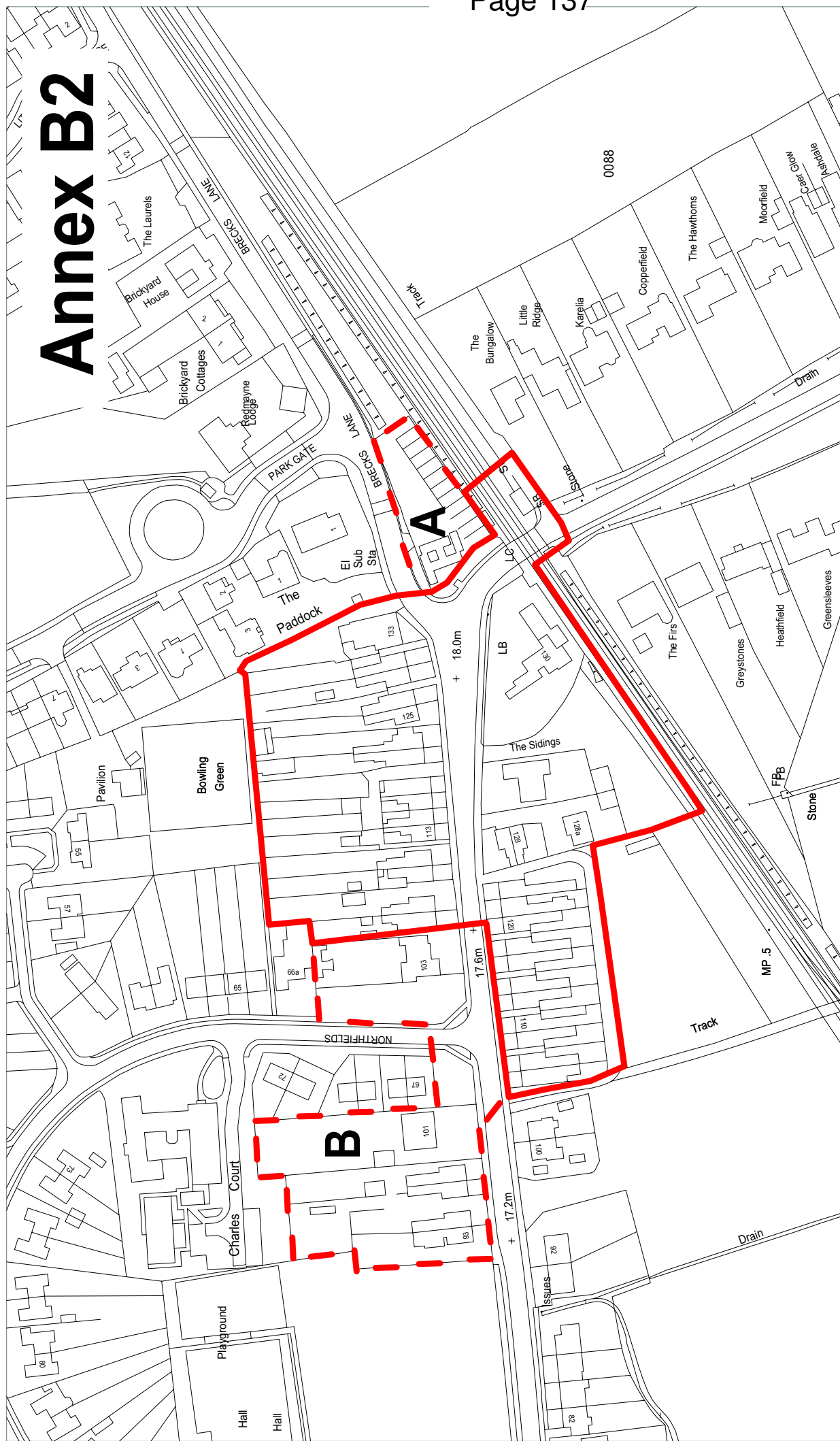
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Project

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Annex B2



STRENSALL RAILWAY BUILDINGS - CONSERVATION AREA 31 -
Proposed Amendments

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DATE: 29/10/2010
Drawing No: CA31PE

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Existing Conservation Area Boundary

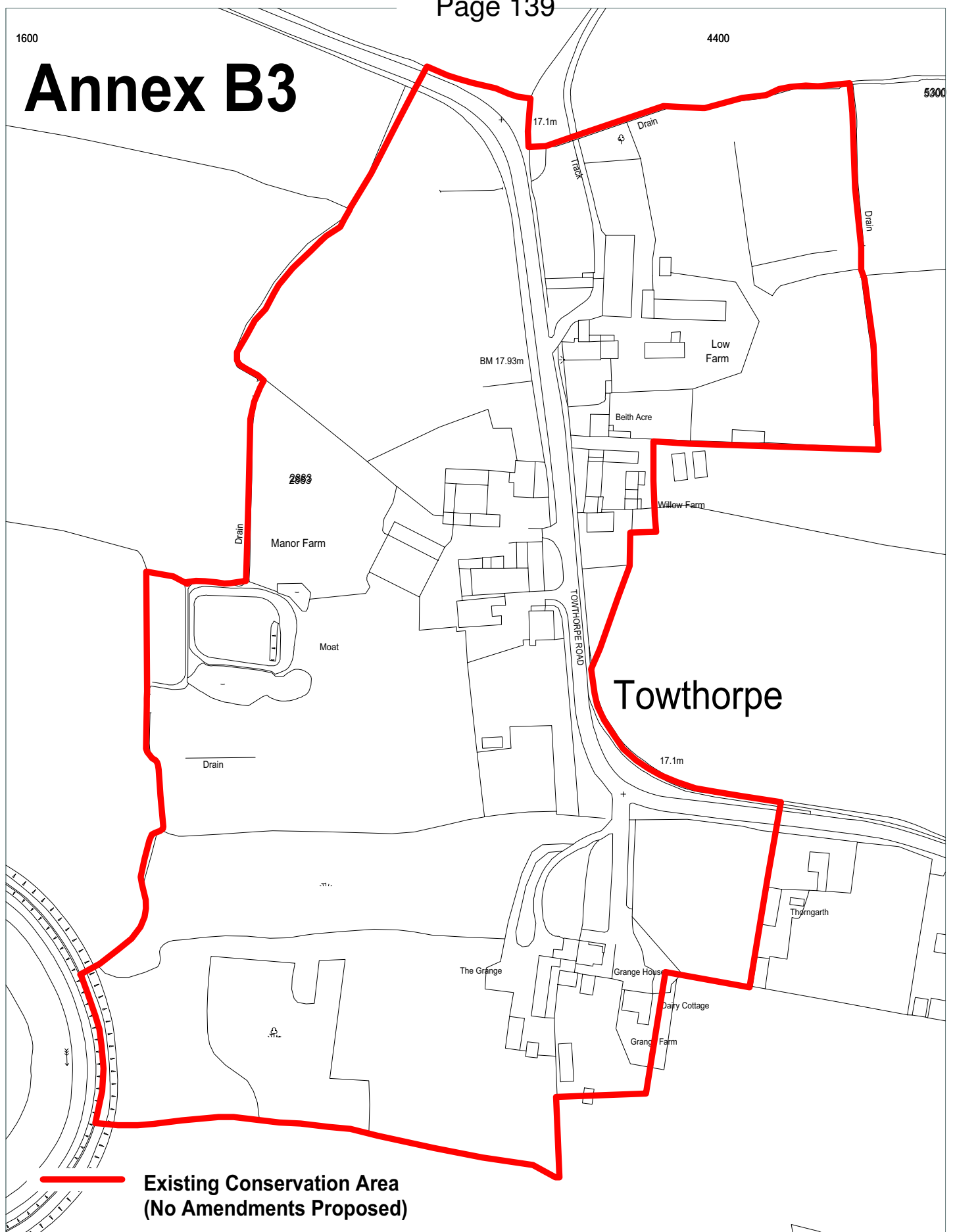
Proposed Amendments to Conservation Area Boundary

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4400

Annex B3



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TOWTHORPE - CONSERVATION AREA 32

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Originating Group

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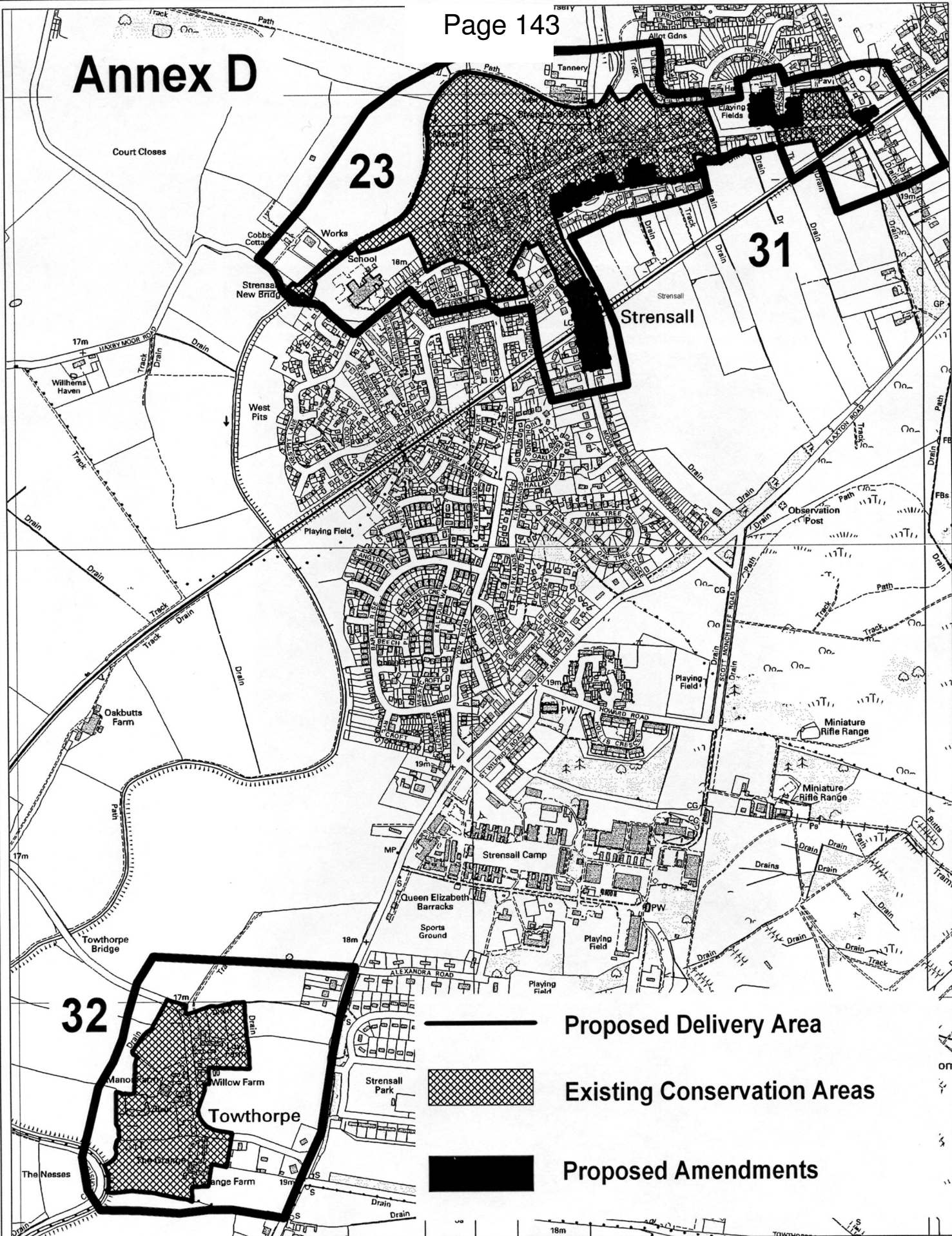
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ANNEX C – Consultee List for Strensall, Strensall Railway Buildings and Towthorpe Conservation Area Character Appraisals, November 2010

NAME	POSITION	ORGANISATION
Cllr M Kirk	Strensall Ward Councillor	City of York Council
Cllr S Wiseman	Strensall Ward Councillor	City of York Council
TBC	Owner Former Station Yard (Strensall Railway Buildings proposed extension)	Network Rail
Susan Nunn	Clerk	Strensall & Towthorpe Parish Council
Val Jesse	Secretary	Strensall Local History Group
June Card	Secretary	River Foss Society
Alison Sinclair	Chair	York Open Planning Forum
Verlie Riley	Coordinator	Conservation Area Advisory Panel
Dr G Woolley	Chair	CPRE (York and Selby District)
Alison Fisher		English Heritage
Steve Roskams		York Archaeological Forum
Peter Brown	Director	York Civic Trust
David Brinklow	Chairman	Yorkshire Architectural & Archaeological Society
Barry Potter	Chair	York Natural Environment Panel
Lynne Walker		Council for British Archaeology
Judy Jones	Heritage Advisor	British Waterways
Mr J Cornell		Railway Heritage Trust
Sir	Coordinator	Network Rail/ East Coast Mainline
Sir	Coordinator	Victorian Society
Sir	Coordinator	Twentieth Century Society
Michael Slater	Assistant Director	CYC City Strategy
Martin Grainger	Principle Forward Planning Officer	CYC City Development
Derek Gauld	Principle Development Officer	CYC City Development
Esther Priestley	Landscape Architect	CYC DCSD
Bob Missin	Countryside Officer	CYC DCSD
John Oxley	City Archaeologist	CYC DCSD
Verlie Riley	Common Land Officer	CYC DCSD
Bob Sydes	Renaissance Team	CYC DCSD
Jonathan Carr	Head of Development Management	CYC Development Management
Simon Glazier	Team Leader Major & Commercial	CYC Development Management
Gareth Arnold	Team Leader Household & Small Scale	CYC Development Management
Ian Stokes	Principle Transport Planner - Strategy	CYC Transport Planning Unit
Howard Watson	Area Engineer	CYC Highway Development
Karen Streeton	Support Services Manager	CYC Development Management
Pete Audin	Head of Local Land Charges	CYC Local Land Charges
Andrew Docherty	Assistant Director of Legal Governance and IT	CYC Legal Services
James Nicholson	Neighbourhood Management Officer	CYC Neighbourhood Management Unit

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Annex D



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CONSERVATION AREAS 23,31 & 32 -
Proposed Delivery Area

SCALE 1:11000

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